

NJDOT FY2021 MUNICIPAL AID PROGRAM

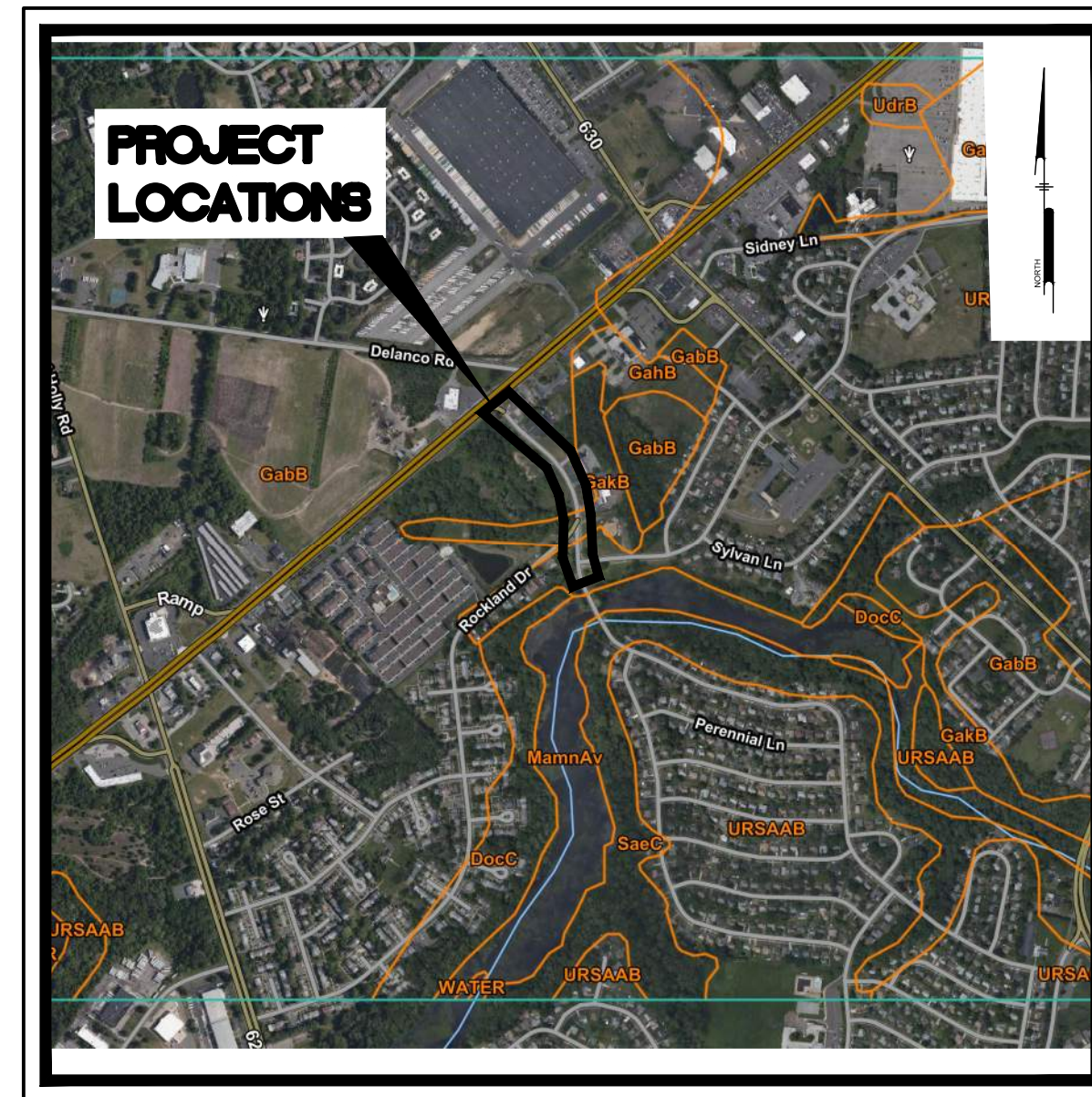
TOWNSHIP OF WILLINGBORO, BURLINGTON COUNTY, NEW JERSEY

PENNYPACKER DRIVE STA. 0+62 TO 14+90

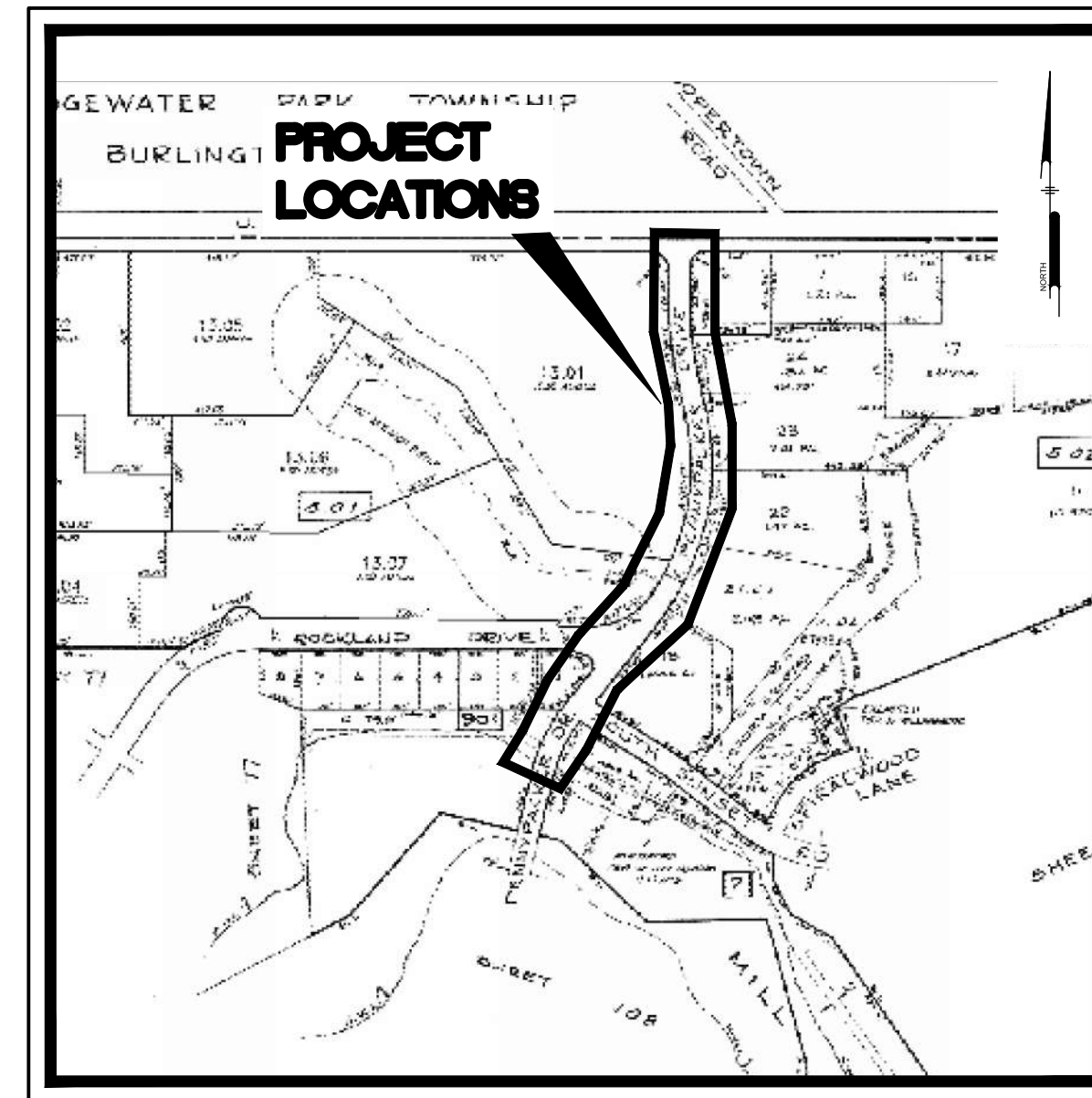
PREPARED FOR:
OWNER

TOWNSHIP OF WILLINGBORO

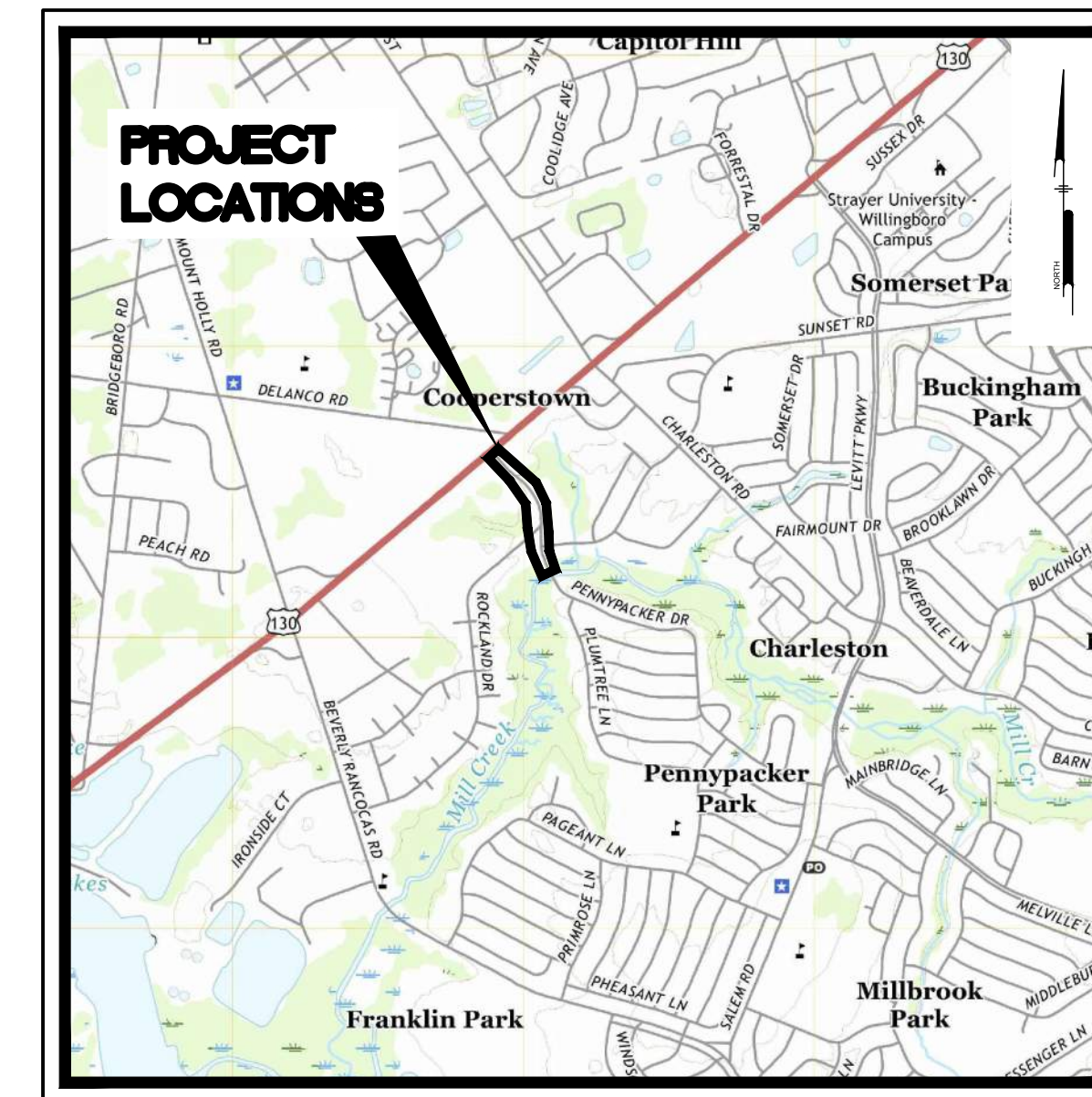
1 REV DR MARTIN LUTHER KING JR DRIVE
WILLINGBORO, NJ 08046



SOILS MAP
Scale: NTS



LOCATION MAP
Scale: NTS



USGS MAP
Scale: 1" = 2000'

PREPARED BY:

PENNONI ASSOCIATES INC.



515 Grove Street, Suite 1B
Haddon Heights, NJ 08035
T 856.547.0505
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NEW JERSEY



CALL BEFORE YOU DIG
1-800-272-1000

CONTRACTOR SHALL BE RESPONSIBLE TO COMPLY WITH THE REQUIREMENTS OF THE NEW JERSEY ONE-CALL DAMAGE PREVENTION SYSTEM AS STATED IN THE "UNDERGROUND FACILITY PROTECTION ACT".

ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR AND OWNER MUST BE NOTIFIED OF ANY DISCREPANCIES BEFORE PROCEEDING WITH WORK

HUGH J. DOUGHERTY
PROFESSIONAL ENGINEER
NEW JERSEY LICENSE NO. GE34634

Hugh Dougherty

11/1/2022

NJDOT FY2021 MUNICIPAL AID PROGRAM
WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

COVER SHEET / KEY PLAN

WILLINGBORO TOWNSHIP
1 REV DR MARTIN LUTHER KING JR RD
WILLINGBORO, NJ 08046

NO.	DATE	REVISIONS	BY
1	11/1/2022	NJDOT REVIEW COMMENTS	SMR

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PROJECT: WTCEX21027
DATE: DECEMBER 14, 2021
DRAWING SCALE: AS NOTED
DRAWN BY: SMR
APPROVED BY: HJD

CM0001

SHEET 1 OF 10

LEGEND

- PROPOSED BASE REPAIR
- PROPOSED CONCRETE
- PROPOSED PAVING
- FENCE
- CURB
- DEPRESSED CURB
- SPOT ELEVATION
- SIGN
- SIGN POST AND BOARD
- CONTOUR
- RIGHT-OF-WAY LINE
- ADD ALTERNATE AND BASE BID MEET
- ALIGNMENT CENTER LINE
- WATER LINE
- SANITARY SEWER
- STORM SEWER
- GAS LINE
- OVERHEAD ELECTRIC
- GAS VALVE
- SANITARY SEWER CLEANOUT
- STORM DRAIN MANHOLE
- STORM DRAIN INLET
- TELEPHONE JUNCTION BOX
- TELEPHONE MANHOLE
- TRAFFIC LIGHT
- TRAFFIC SIGNAL PEDESTAL
- TRAFFIC JUNCTION BOX
- TELEPHONE PEDESTAL
- FIRE HYDRANT
- UTILITY POLE
- WATER VALVE
- UNIDENTIFIED MANHOLE

GENERAL NOTES:

- CURB RAMPS AND SIDEWALKS THAT ARE CONSTRUCTED NOT IN COMPLIANCE WITH ADA ACCESSIBILITY REQUIREMENTS INCLUDING MAXIMUM RUNNING SLOPES, MAXIMUM CROSS SLOPES, LANDING AREA LOCATION AND SIZE, DIVERGING SIDEWALK DROP OFF PROTECTION, BLENDED TRANSITIONS, BLENDED TRANSITION RUNNING SLOPES AND CROSS SLOPES, FLARE WIDTHS, GRADE BREAKS AND GRADE BREAK DIRECTIONS, CLEAR SPACE SIZE AND LOCATION, WILL NOT BE ACCEPTED.
- NJDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2019 AND CURRENT AND SUPPLEMENTARY SPECIFICATIONS THERETO GOVERN.
- SUPPLEMENTARY SPECIFICATIONS FOR STATE AID PROJECTS APPLY.
- REFER TO CURRENT "NJDOT STANDARD ROADWAY CONSTRUCTION-TRAFFIC CONTROL-BRIDGE CONSTRUCTION DETAILS 2019" FOR ADDITIONAL GUIDANCE.
- ALL DIMENSIONS, QUANTITIES AND EXISTING PHYSICAL CHARACTERISTICS OF THE WORK AREA SHALL BE REVIEWED AND FIELD VERIFIED BY THE CONTRACTOR PRIOR TO SUBMITTING A BID, MATERIAL ORDERING OR FABRICATION OF ANY PART OF THE WORK. ANY DISCREPANCIES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE ENGINEER.
- PRIOR TO THE START OF ANY OF THE CONSTRUCTION THE CONTRACTOR SHALL CALL THE "ONE CALL" NUMBER (1-800-272-1000) FOR UTILITY MARKOUT LOCATION AND NOTIFICATION. THE CONFIRMATION NUMBER SHALL BE GIVEN TO THE ENGINEER PRIOR TO THE COMMENCEMENT OF THE CONSTRUCTION.
- IT IS THE OBLIGATION AND COST OF THE CONTRACTOR TO CONDUCT HIS OWN SUBSURFACE INVESTIGATIONS AS HE MAY DEEM NECESSARY. THIS INVESTIGATION SHALL BE COMPLETED PRIOR TO BID SUBMITTAL TO DETERMINE ANY UNIQUE CONDITIONS OR MATERIALS THAT MAY EXIST WHICH COULD EFFECT THE CONSTRUCTION OR COST. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY SUCH FINDINGS.
- EXPANSION JOINT MATERIAL SHALL BE 1/2" THICK PREMOLDED BITUMINOUS IMPREGNATED MATERIAL FOR THE FULL DEPTH OF THE THINNER MATERIAL. COST FOR THIS ITEM SHALL BE INCLUDED IN THE UNIT COST OF EITHER SIDEWALK, CURB, OR APRON, WHICHEVER APPROPRIATE, IN THE BID PROPOSAL.
- WHERE NEW SIDEWALK IS TO BE INSTALLED ADJACENT TO EXISTING SIDEWALK, DRIVEWAY, POLE, ETC., AN EXPANSION JOINT SHALL BE PLACED BETWEEN EACH AND THE NEW SIDEWALK ELEVATION SHALL MATCH THAT OF THE EXISTING IMPROVEMENT.
- EXISTING ITEMS NOTED ON THE PLANS FOR REMOVAL ARE INTENDED TO BEGIN AND TERMINATE AT EXISTING JOINTS HOWEVER, WHERE A JOINT DOES NOT EXIST THEN THE CONTRACTOR WILL BE REQUIRED TO "SAW CUT" TO MAKE AN EVEN, STRAIGHT LINE SEPARATION. THE COST OF THIS ITEM SHALL BE INCLUDED IN THE COST FOR EACH UNIT LISTED IN THE APPROPRIATE BID PROPOSAL ITEM.
- EXTREME CARE SHALL BE EXERCISED BY THE CONTRACTOR WHEN REMOVING EXISTING SIDEWALK, CURB OR DRIVEWAY MATERIAL SO AS NOT TO DISTURB ADJACENT IMPROVEMENTS. ADJACENT IMPROVEMENTS DISTURBED DURING REMOVAL OF THE NOTED ITEMS SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR.
- ALL CONCRETE SHALL BE 28 DAY AIR ENTRAINED (3% TO 5%) CONCRETE. CONCRETE COMPRESSIVE STRENGTH OF (F'C) 4000 PSI MINIMUM SHALL BE DEVELOPED UNLESS NOTED OTHERWISE.
- ALL EDGES (VERTICAL/HORIZONTAL) BETWEEN EXISTING PAVEMENT OR NEW/EXISTING CURB AND NEW PAVEMENT SHALL HAVE TACKCOAT APPLIED. COST FOR THIS ITEM SHALL BE INCLUDED IN THE UNIT COST FOR HMA SURFACE COURSE LISTED IN THE BID PROPOSAL.
- MAINTENANCE AND PROTECTION OF TRAFFIC (MPT) AS DEFINED AND DESCRIBED IN THE SPECIFICATION'S SHALL BE STRICTLY FOLLOWED. A DETOUR PLAN SHALL NOT BE PERMITTED. COST FOR THIS ITEM SHALL BE INCLUDED IN THE COST FOR EACH UNIT LISTED IN THE BID PROPOSAL.
- SANITARY SEWER CLEANOUTS IN CONCRETE REPLACEMENT/INSTALLATION AREAS SHALL RECEIVE A CAST IRON STREET BOX WITH "SEWER" LETTERED IN THE UID.
- EXISTING UTILITIES INCLUDING PRIVATE LAWN SPRINKLER HEADS, WATER METER(S), VALVE BOXES, CLEAN OUTS, CURB STOPS, ETC. SHALL BE ADJUSTED TO MATCH THE FINISHED GRADE BY THE CONTRACTOR; COST FOR THIS ITEM SHALL BE INCLUDED IN AND DISTRIBUTED AMONG THE ITEMS IN THE BID PROPOSAL. WHERE UTILITIES OCCUR IN THE CONCRETE AREAS, THE ITEM SHALL BE EITHER "BOXED OUT" AND GROUTED WITH NON-SHRINK GROUT OR RAISED TO FINISHED GRADE THEN THE CONCRETE POURED. THE END RESULT REQUIRED IS THAT NO CHANGE IN ELEVATION SHALL REMAIN AFTER THE CONSTRUCTION IS COMPLETED.
- SIDEWALK AREAS THAT SHALL BE REMOVED AND REPLACED ARE GENERALLY NOTED PHYSICALLY WITH A PAINT SPLATCH. PRIOR TO THE REMOVAL OF ANY SIDEWALK THE CONTRACTOR SHALL VERIFY WITH THE ENGINEER OR HIS REPRESENTATIVE THE EXACT EXTENT AND QUANTITY.
- CONCRETE SIDEWALK ADJACENT TO PROPOSED HANDICAP RAMPS SHALL BE REPLACED AS NEEDED TO PROVIDE AN ADA-COMPLIANT RAMP.
- CONTRACTOR IS RESPONSIBLE FOR SAW CUTTING ANY EXISTING CONCRETE (EX: FACE FORM POURS) THAT PROTRUDES INTO NEW ROADWAY.
- CONTRACTOR SHALL ENSURE THAT STORMWATER WILL NOT BECOME TRAPPED AT EXISTING DRIVEWAY APRONS.

LEGEND

- EXISTING BUILDING
- EXISTING FENCE
- EXISTING CURB
- EXISTING DEPRESSED CURB
- EXISTING SIDEWALK
- EXISTING CONCRETE
- EXISTING MILLOVERLAY PAVEMENT
- EXISTING FULL DEPTH PAVEMENT REPLACEMENT/BASE REPAIR
- EXISTING GRASS INFILL AREA
- EXISTING INLET PROTECTION/REPAIR
- EXISTING LIMIT OF WORK AREA
- EXISTING SPOT ELEVATION
- EXISTING IRRIGATION HEAD
- EXISTING LIGHT
- EXISTING BOLLARD
- EXISTING SIGN
- EXISTING SIGN POST AND BOARD
- EXISTING CABLE TV JUNCTION BOX
- EXISTING CABLE TV MANHOLE
- EXISTING COMMUNICATIONS MANHOLE
- EXISTING COMMUNICATIONS HANDHOLE
- EXISTING FIBER HANDHOLE
- EXISTING FIBER JUNCTION BOX
- EXISTING FIBER MANHOLE
- EXISTING GAS MANHOLE
- EXISTING GAS METER
- EXISTING GAS STUB OUT
- EXISTING GAS VALVE
- EXISTING ELECTRICAL HANDHOLE
- EXISTING ELECTRICAL METER
- EXISTING ELECTRICAL MANHOLE
- EXISTING ELECTRICAL PANEL BOX
- EXISTING ELECTRICAL PEDESTAL
- EXISTING ELECTRICAL STUB OUT
- EXISTING ELECTRICAL STUB OUT
- EXISTING MONITORING WELL
- EXISTING SANITARY SEWER CLEANOUT
- EXISTING SANITARY SEWER CLEANOUT
- EXISTING SANITARY MANHOLE
- EXISTING STEAM MANHOLE
- EXISTING STORM DRAIN CLEANOUT
- EXISTING STORM DRAIN MANHOLE
- EXISTING STORM DRAIN FLARED END SECTION
- EXISTING STORM DRAIN HEADWALL
- EXISTING STORM DRAIN ID
- EXISTING STORM DRAIN INLET
- EXISTING STORM ROOF DRAIN
- EXISTING STORM STAND PIPE
- EXISTING STORM DRAIN WING WALL
- PROPOSED BUILDING
- PROPOSED FENCE
- PROPOSED CURB
- PROPOSED DEPRESSED CURB
- PROPOSED SIDEWALK
- PROPOSED CONCRETE
- PROPOSED MILLOVERLAY PAVEMENT
- PROPOSED FULL DEPTH PAVEMENT REPLACEMENT/BASE REPAIR
- PROPOSED GRASS INFILL AREA
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- PROPOSED STORM DRAIN ID
- PROPOSED STORM DRAIN INLET
- PROPOSED STORM ROOF DRAIN
- PROPOSED STORM STAND PIPE
- PROPOSED STORM DRAIN WING WALL

LEGEND

- EXISTING TELEPHONE HANDHOLE
- EXISTING TELEPHONE JUNCTION BOX
- EXISTING TELEPHONE MANHOLE
- EXISTING TELEPHONE PANEL BOX
- EXISTING TELEPHONE PEDESTAL
- EXISTING EMERGENCY CALL STATION
- EXISTING SEPTIC TANK
- EXISTING UTILITY POLE
- EXISTING BUSH
- EXISTING CONIFEROUS TREE
- EXISTING DECIDUOUS TREE
- EXISTING STUMP
- EXISTING FIRE HYDRANT
- EXISTING WATER MANHOLE
- EXISTING WATER POST INDICATOR VALVE
- EXISTING SIAMSESE / FIRE DEPARTMENT CONNECTION
- EXISTING WATER STUB OUT
- EXISTING WATER VALVE
- EXISTING WATER METER
- EXISTING CURB BOX
- EXISTING UNIDENTIFIED HANDHOLE
- EXISTING UNIDENTIFIED JUNCTION BOX
- EXISTING UNIDENTIFIED MANHOLE
- EXISTING UNIDENTIFIED PANEL BOX
- EXISTING UNIDENTIFIED PEDESTAL
- EXISTING UNIDENTIFIED VALVE
- PROPOSED TELEPHONE HANDHOLE
- PROPOSED TELEPHONE JUNCTION BOX
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- PROPOSED WATER METER
- PROPOSED CURB BOX
- PROPOSED UNIDENTIFIED HANDHOLE
- PROPOSED UNIDENTIFIED JUNCTION BOX
- PROPOSED UNIDENTIFIED MANHOLE
- PROPOSED UNIDENTIFIED PANEL BOX
- PROPOSED UNIDENTIFIED PEDESTAL
- PROPOSED UNIDENTIFIED VALVE

Item #	Description	Quantity
Pennypacker Drive		
1	Variable Depth Mill, Minimum 2" Thick	5,794 SY
2	HMA 9.5M64 Surface Course, 2" Thick	5,794 SY
3	Base Repair, 8" Thick	581 SY
4	Detectable Warning Surface	32 SF
5	Vertical Concrete Curb (Remove and Replace, If and Where Directed)	419 LF
6	Concrete Sidewalk, 4" Thick	36 SY
7	Concrete Drive apron, 8" Thick	120 SY
8	Concrete Slope Gutter, 8" Thick	58 LF
9	Concrete Swale, 8" Thick	320 SF
10	Replace "B" Inlet Casting with "ECO" Head and Bicycle-safe grate (Includes Concrete and Asphalt Restorations)	2 UN
11	Thermoplastic, Double Yellow, 4" Wide	1,312 LF
12	Thermoplastic, White, 8" Wide	164 LF
13	Thermoplastic, White, 24" Wide	17 LF
14	Relocate Traffic Sign	1 UN
15	Allowance for Asphalt Price Adjustment for Hot Mix Asphalt, if needed	1 LS

SHEET #	CM	TITLE	DATE	LAST REVISED
1	CM0001	COVER SHEET/KEY PLAN	12/14/2021	1/11/2022
2	CM0002	GENERAL NOTES	12/14/2021	1/11/2022
3	CM1001	PENNYPACKER - SITE PLAN 1	12/14/2021	1/11/2022
4	CM1002	PENNYPACKER - SITE PLAN 2	12/14/2021	1/11/2022
5	CM1003	PENNYPACKER - ADA GRADING PLAN	12/14/2021	1/11/2022
6	CM6001	CONSTRUCTION DETAILS	12/14/2021	1/11/2022
7	CM6002	CONSTRUCTION DETAILS	12/14/2021	1/11/2022
8	CM6501	TRAFFIC CONTROL NOTES	12/14/2021	1/11/2022
9	CM6502	TRAFFIC CONTROL DETAILS	12/14/2021	1/11/2022
10	CM8501	SOIL EROSION AND SEDIMENT CONTROL PLAN	12/14/2021	1/11/2022

Pennoni ASSOCIATES INC.
 515 Grove Street, Suite 1B
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 NJ COA. NO. GA28033300

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HUGH J. DOUGHERTY
 PROFESSIONAL ENGINEER
 NEW JERSEY LICENSE NO. GE34634
 1/11/2022

NJDOT FY2021 MUNICIPAL AID PROGRAM
 WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

GENERAL NOTES

WILLINGBORO TOWNSHIP
 1 REV DR MARTIN LUTHER KING JR RD
 WILLINGBORO, NJ 08046

NO.	DATE	REVISIONS	BY
1	1/11/2022	NJDOT REVIEW COMMENTS	SMR

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PROJECT: **WTCEX21027**

DATE: **DECEMBER 14, 2021**

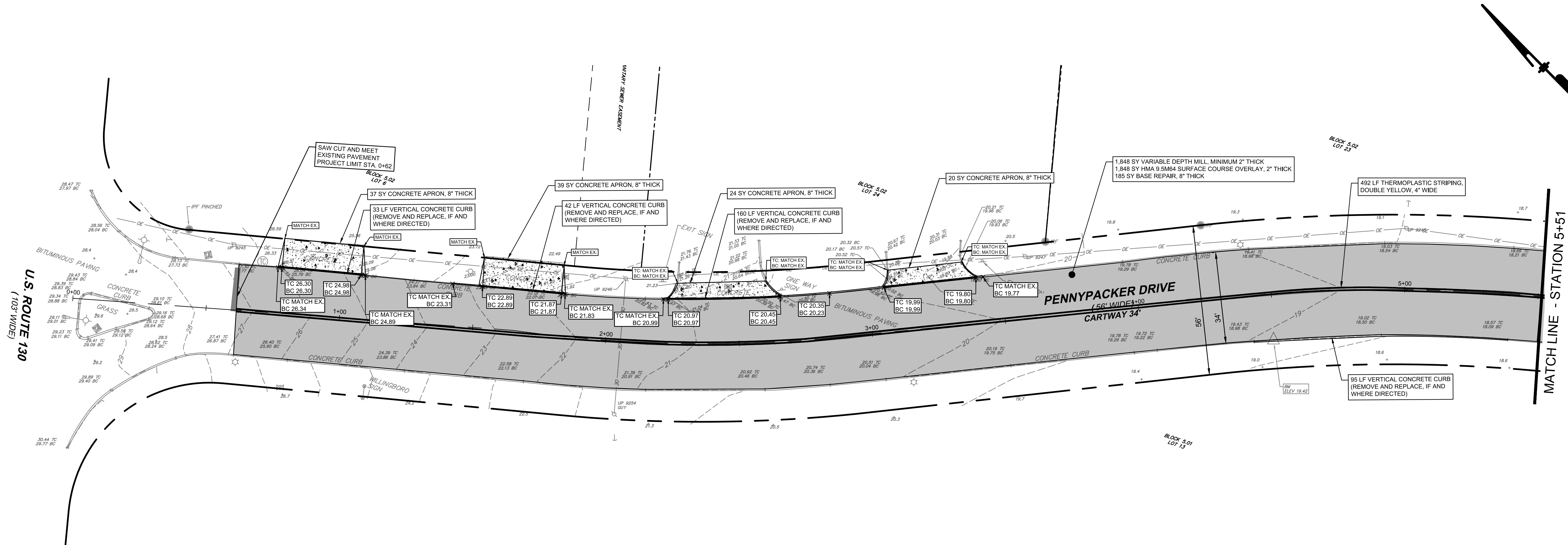
DRAWING SCALE: **AS NOTED**

DRAWN BY: **SMR**

APPROVED BY: **HJD**

CM0002

SHEET **2** OF **10**

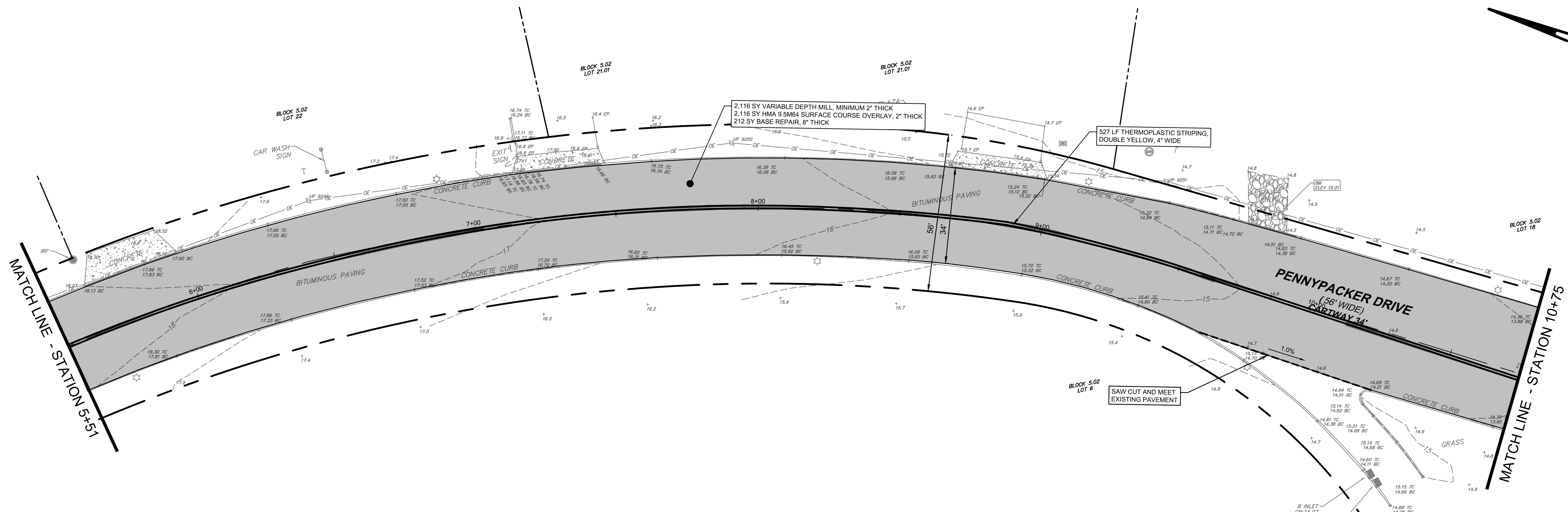


PENNYPACKER DRIVE STA 0+61 TO STA 5+51

SCALE: 1" = 20'

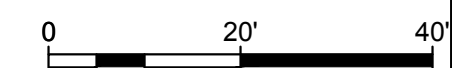
NOTE:

- FOR QUANTITIES REGARDING HANDICAP RAMP IMPROVEMENTS SEE BLOW UP DETAILS (TYP.)



PENNYPACKER DRIVE STA 5+51 TO STA 10+75

SCALE: 1" = 20'



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Hugh Dougherty

11/1/2022

NJDOT FY2021 MUNICIPAL AID PROGRAM
WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

PENNYPACKER - SITE PLAN 1

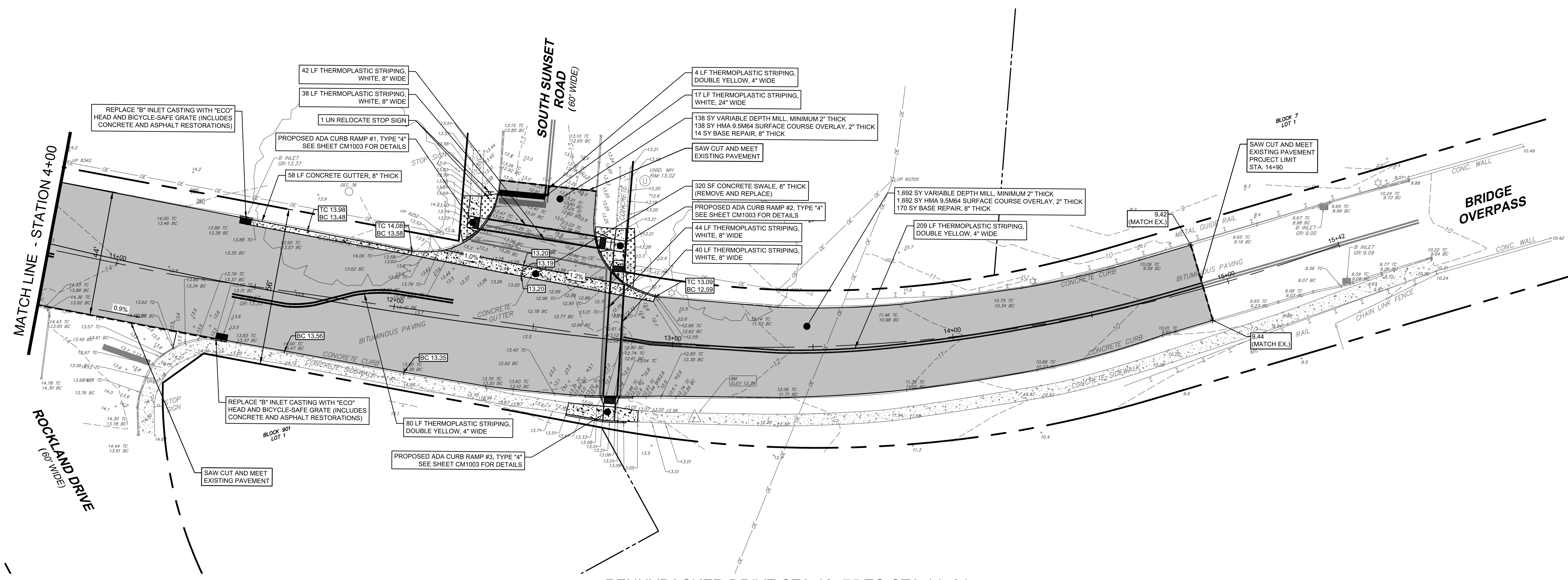
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1 REV DR MARTIN LUTHER KING JR RD
WILLINGBORO, NJ 08046

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PROJECT	WTCEX21027
DATE	DECEMBER 14, 2021
DRAWING SCALE	1"=20'
DRAWN BY	SMR
APPROVED BY	HJD

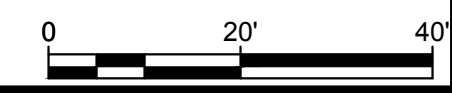
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 PLOTTED: 11/02/22 10:07 AM BY: Sami Rahmy PLOTTSTYLE: Pennoni\NCS.dwg PROJECT STATUS:

U:\Account\WTC\WTC21027 - Rebranding Pennypacker Drive - Phase 1\DESIGN_SHEETS\CM1002.dwg
 PLOTTED: 11/10/2021 10:38 AM BY: Sami Rabbey PLOTTSTYLE: Pennon NCS.ctb PROJECT STATUS:



PENNYPACKER DRIVE STA 10+75 TO STA 14+91

SCALE: 1" = 20'



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 NEW JERSEY LICENSE NO. GE34634

Hugh Dougherty

NJDOT FY2021 MUNICIPAL AID PROGRAM
 WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

PENNYPACKER - SITE PLAN 2

WILLINGBORO TOWNSHIP
 1 REV DR MARTIN LUTHER KING JR RD
 WILLINGBORO, NJ 08046

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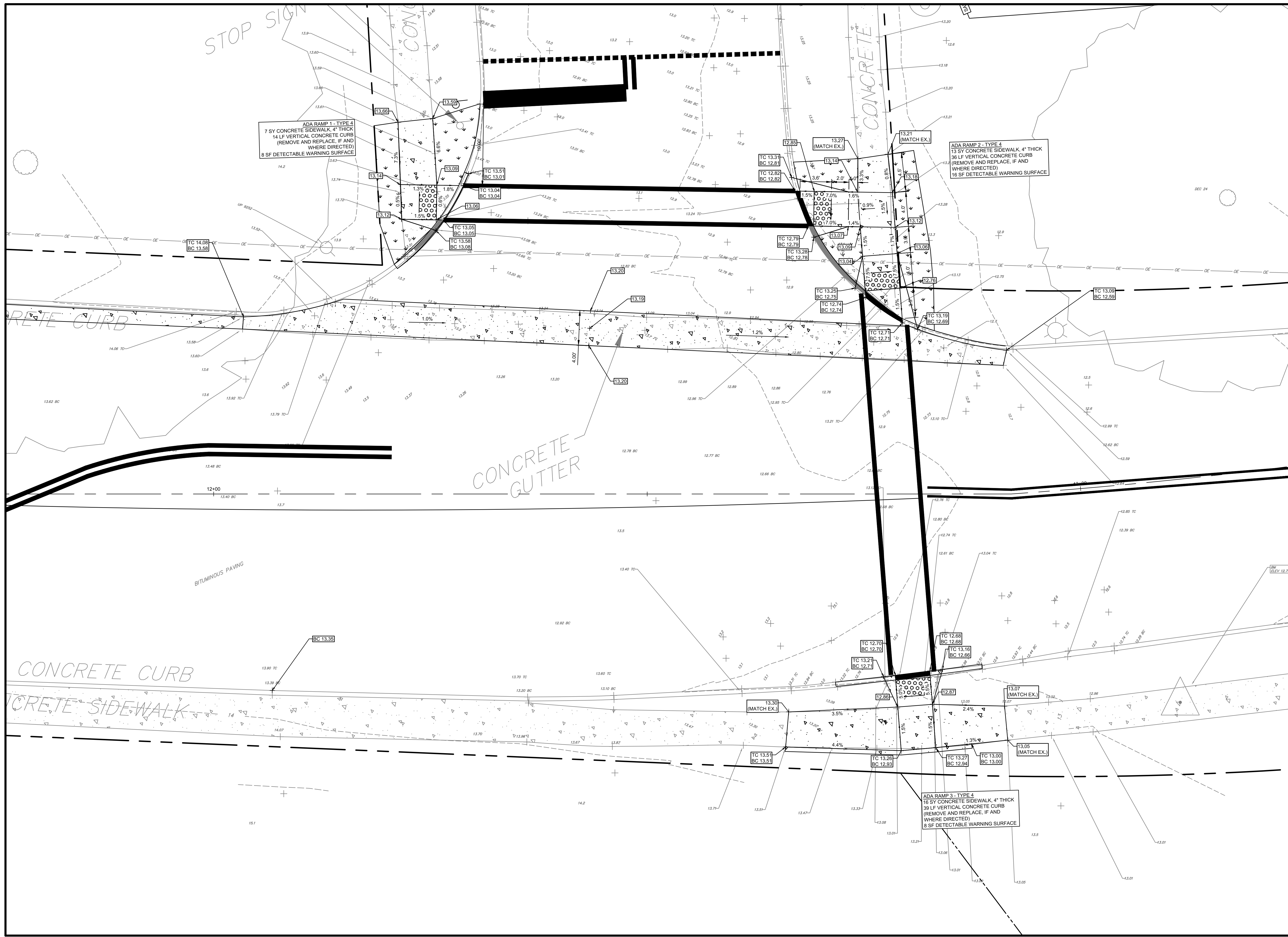
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PROJECT: WTCX21027
 DATE: DECEMBER 14, 2021
 DRAWING SCALE: 1"=20'
 DRAWN BY: SMR
 APPROVED BY: HJD

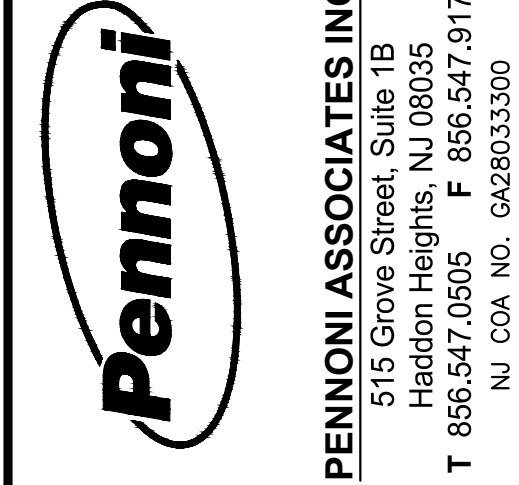
CM1002
 SHEET 4 OF 10

PENNONI ASSOCIATES INC.
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 NJ COA. NO. G428033300

U:\Projects\CM1003\Drawings\Penpacker\Drawings\Penpacker ADA Ramp Details.dwg
 PLOTTED: 11/11/2021 10:38 AM BY: Sam Raley
 PROJECT STATUS:



PENNYPACKER ADA RAMP DETAILS
 SCALE: 1" = 5'



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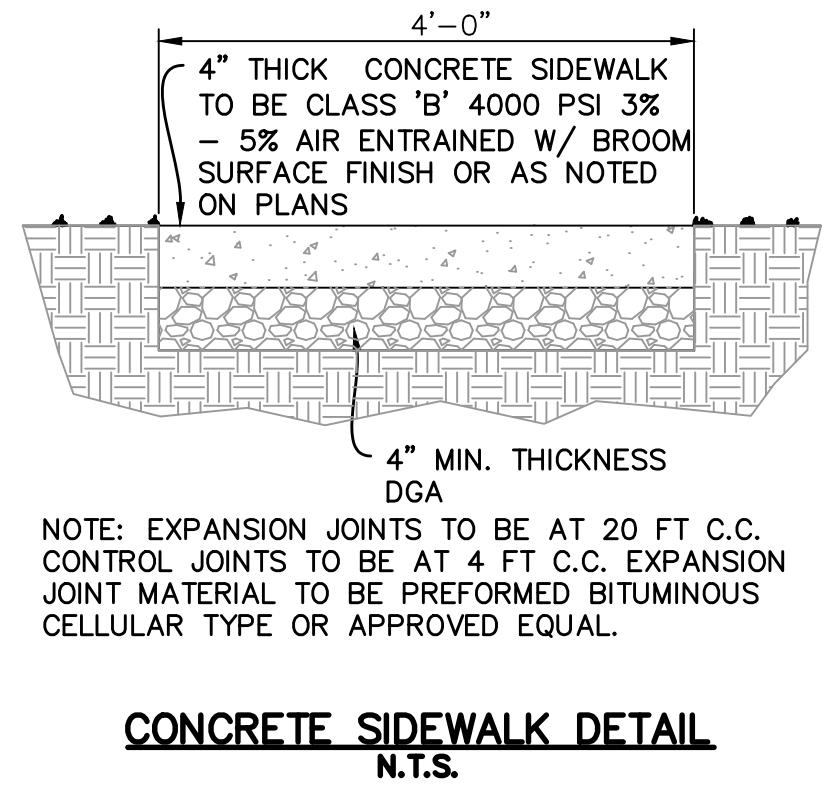
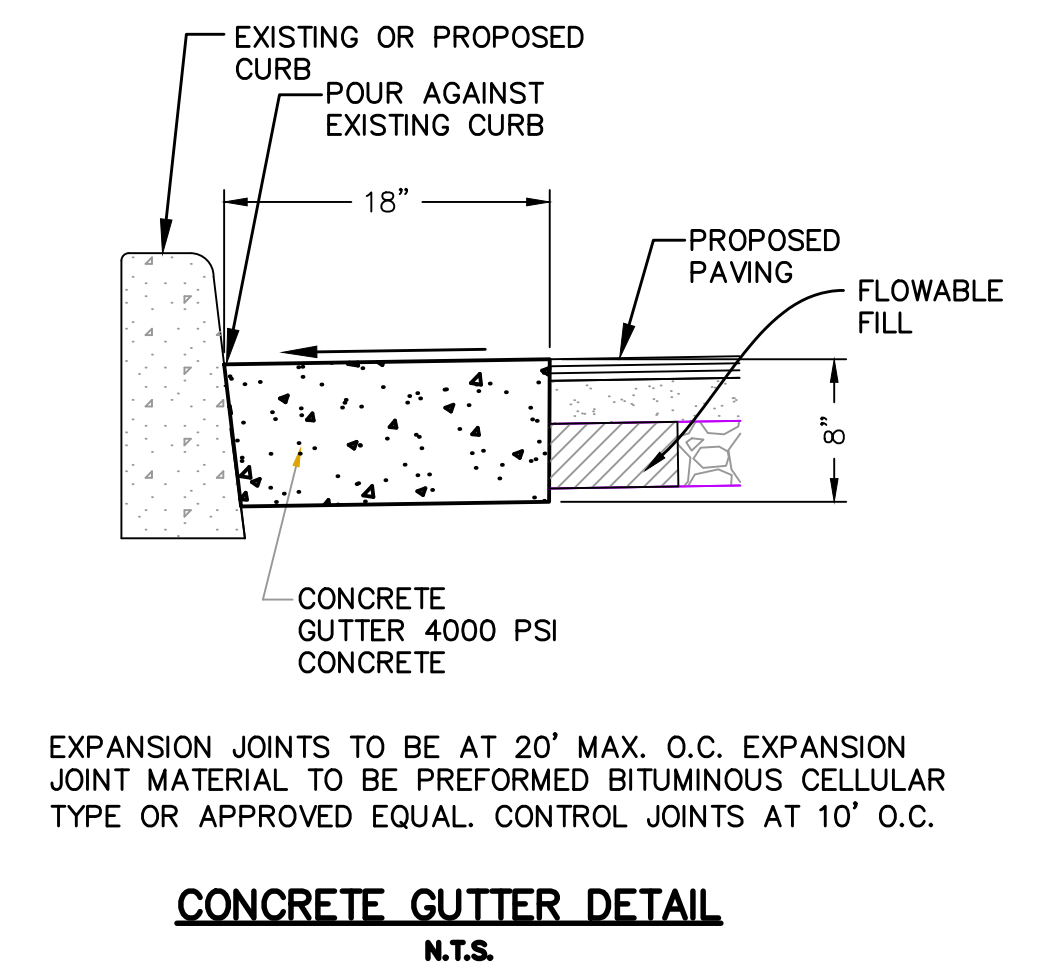
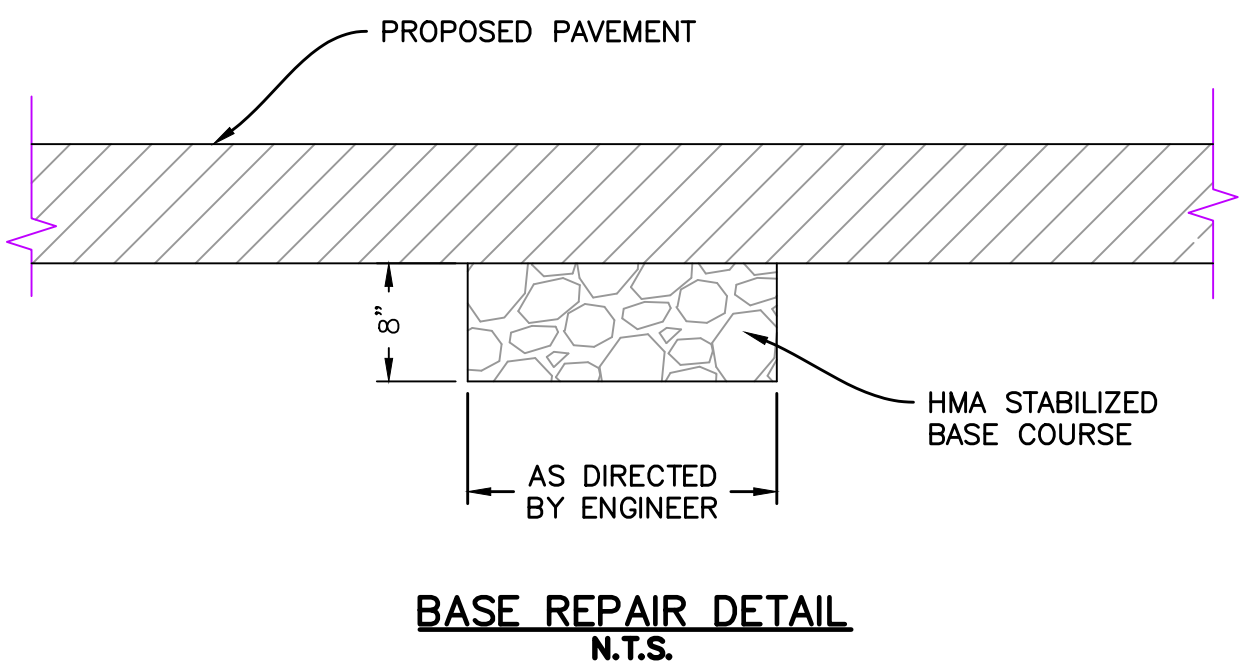
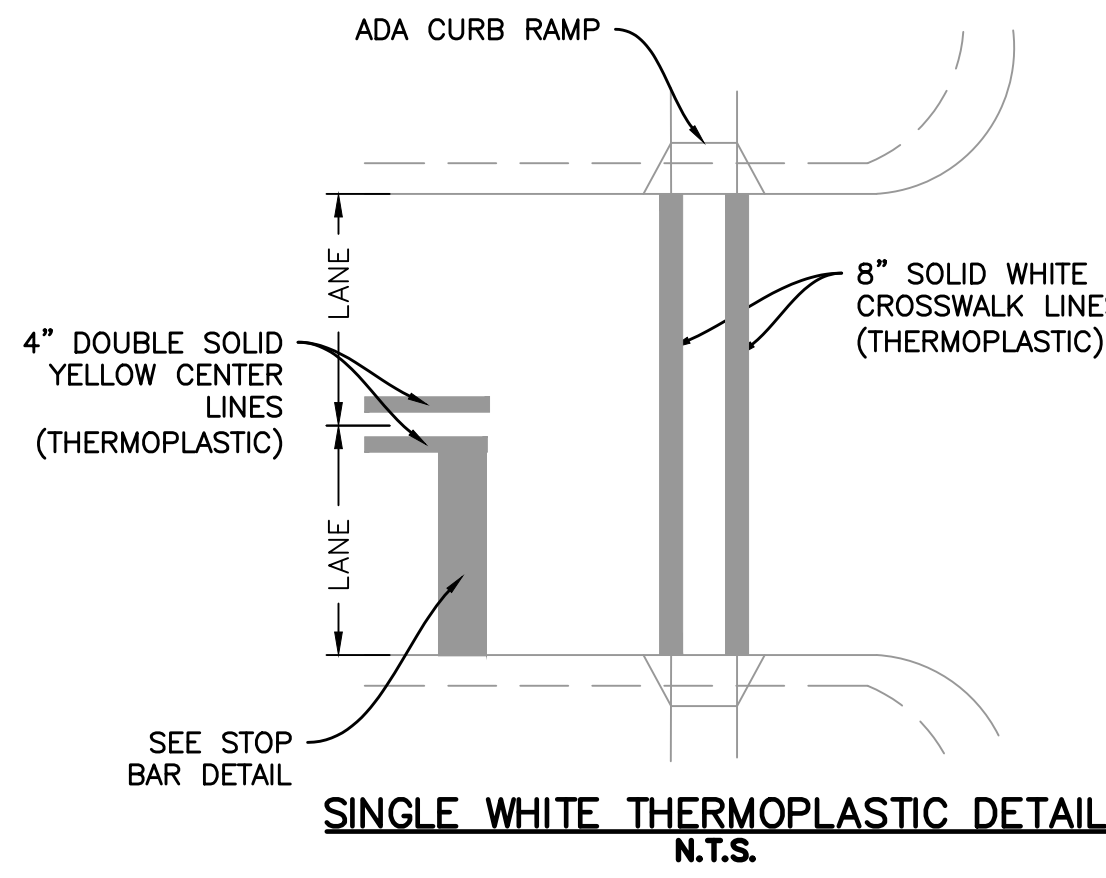
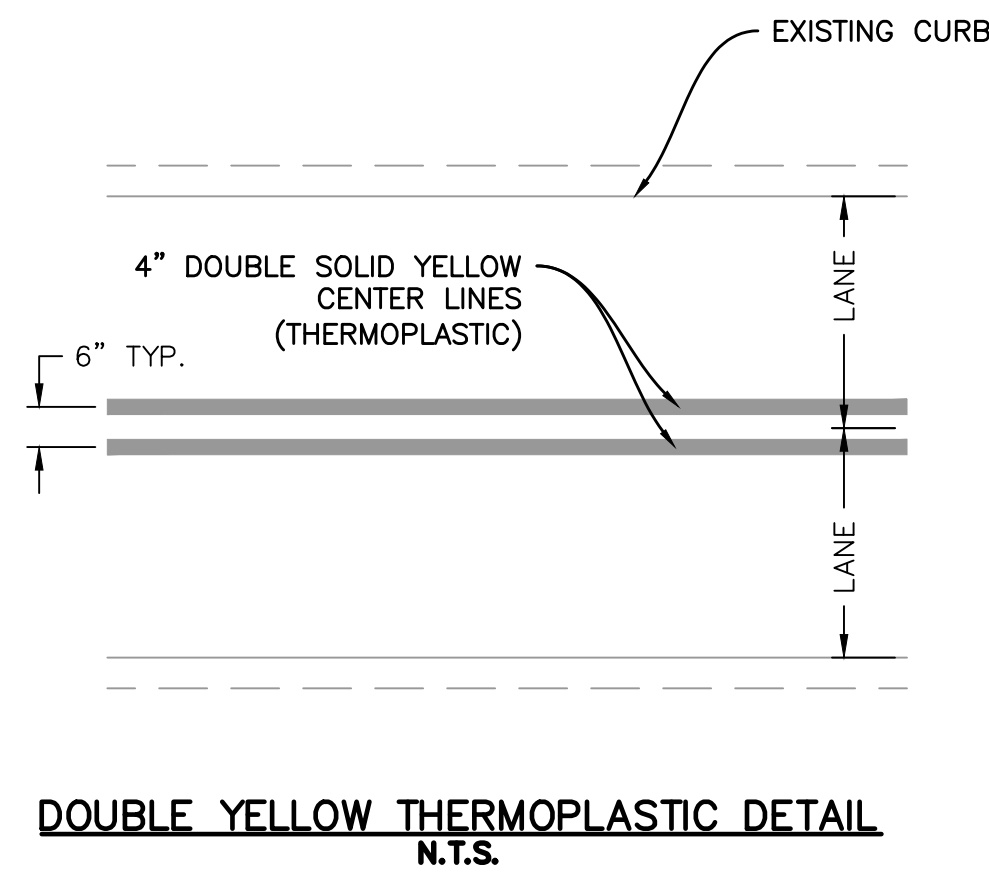
ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR AND OWNER MUST BE NOTIFIED OF ANY DISCREPANCIES BEFORE PROCEEDING WITH WORK
HUGH J. DOUGHERTY
 PROFESSIONAL ENGINEER
 NEW JERSEY LICENSE NO. GE34634
Hugh Dougherty

NJDOT FY2021 MUNICIPAL AID PROGRAM
 WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY
PENNYPACKER - ADA GRADING PLAN
 WILLINGBORO TOWNSHIP
 1 REV DR MARTIN LUTHER KING JR RD
 WILLINGBORO, NJ 08046

NO.	DATE	REVISIONS	SMR	BY
1	11/11/2021	NJDOT REVIEW COMMENTS	SMR	

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PROJECT: **WTCEX21027**
 DATE: **DECEMBER 14, 2021**
 DRAWING SCALE: **1"=20'**
 DRAWN BY: **SMR**
 APPROVED BY: **HJD**



EXPANSION JOINTS TO BE AT 20' MAX. O.C. EXPANSION JOINT MATERIAL TO BE PERFORMED BITUMINOUS CELLULAR TYPE OR APPROVED EQUAL. CONTROL JOINTS AT 10' O.C.

NOTE: EXPANSION JOINTS TO BE AT 20 FT C.C. CONTROL JOINTS TO BE AT 4 FT C.C. EXPANSION JOINT MATERIAL TO BE PERFORMED BITUMINOUS CELLULAR TYPE OR APPROVED EQUAL.

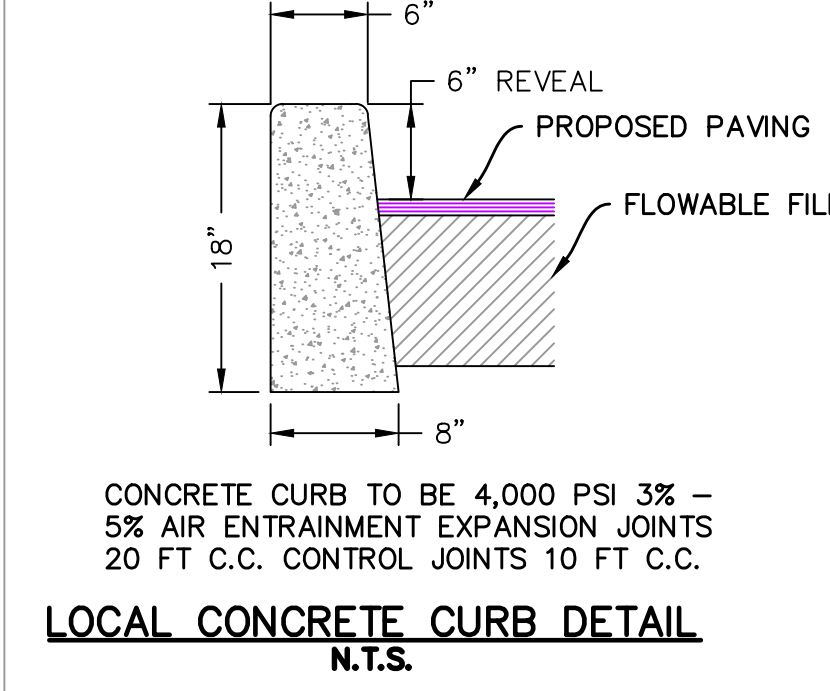
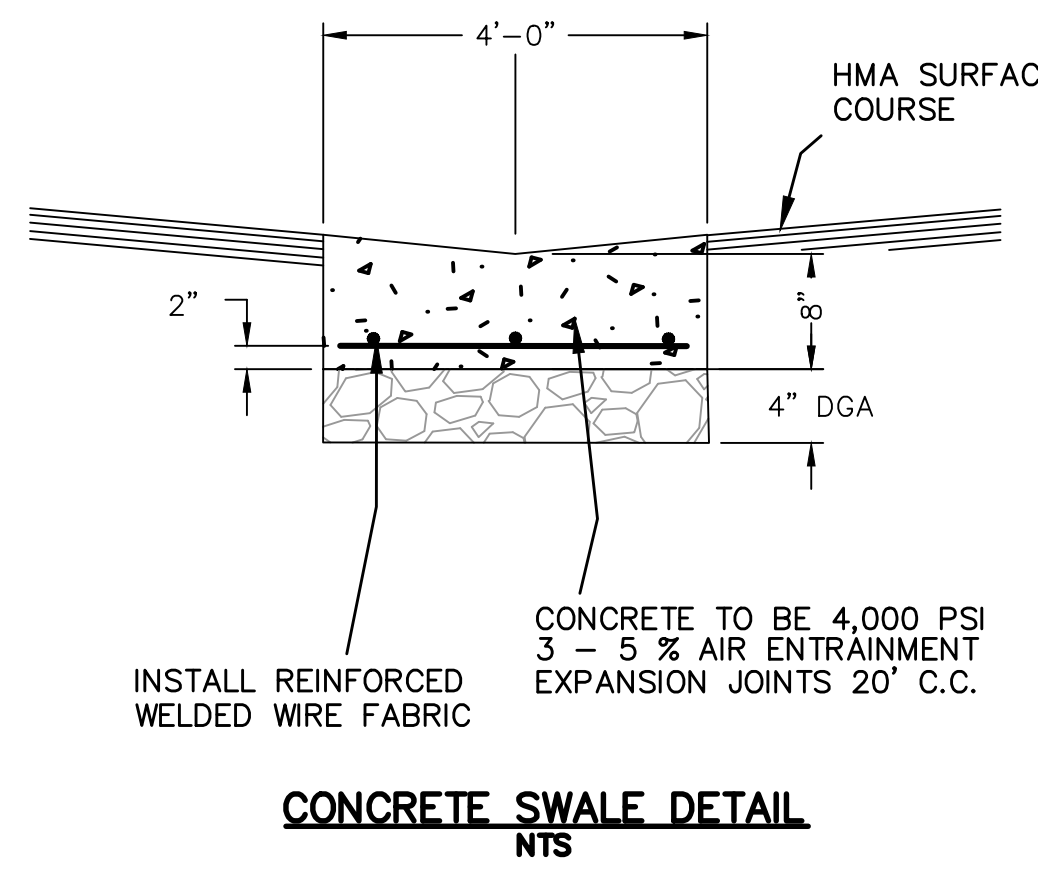
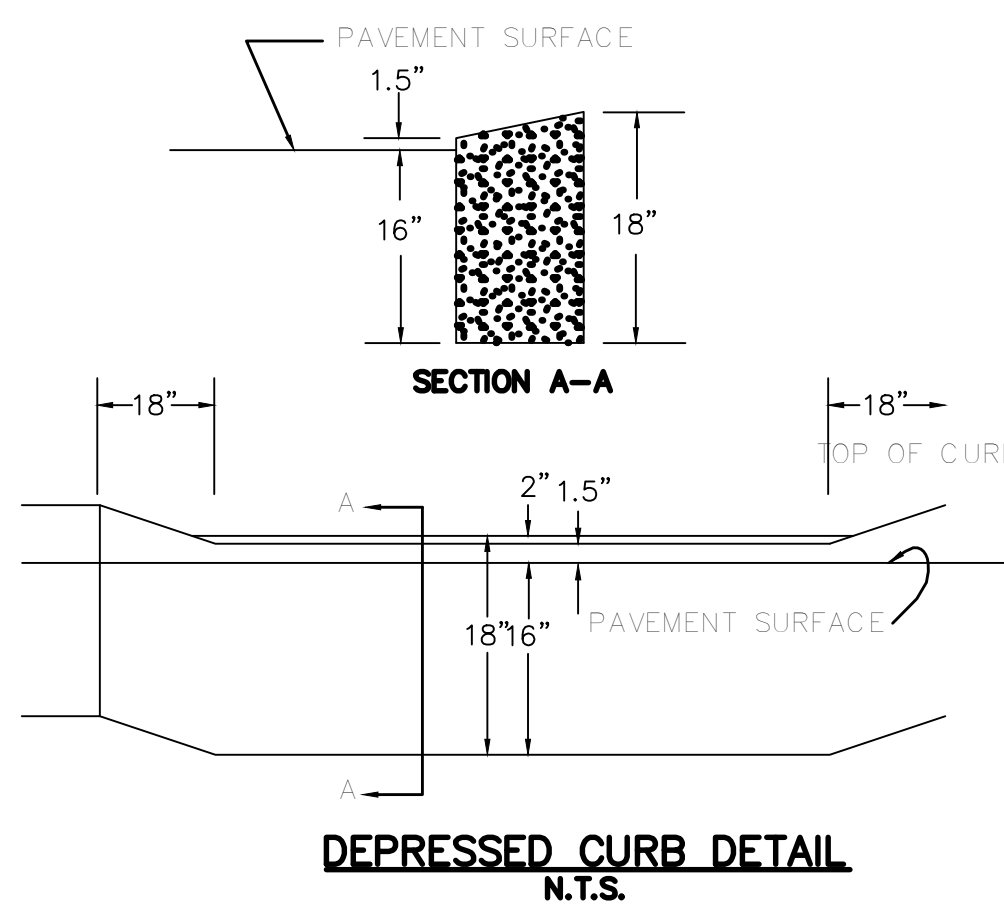
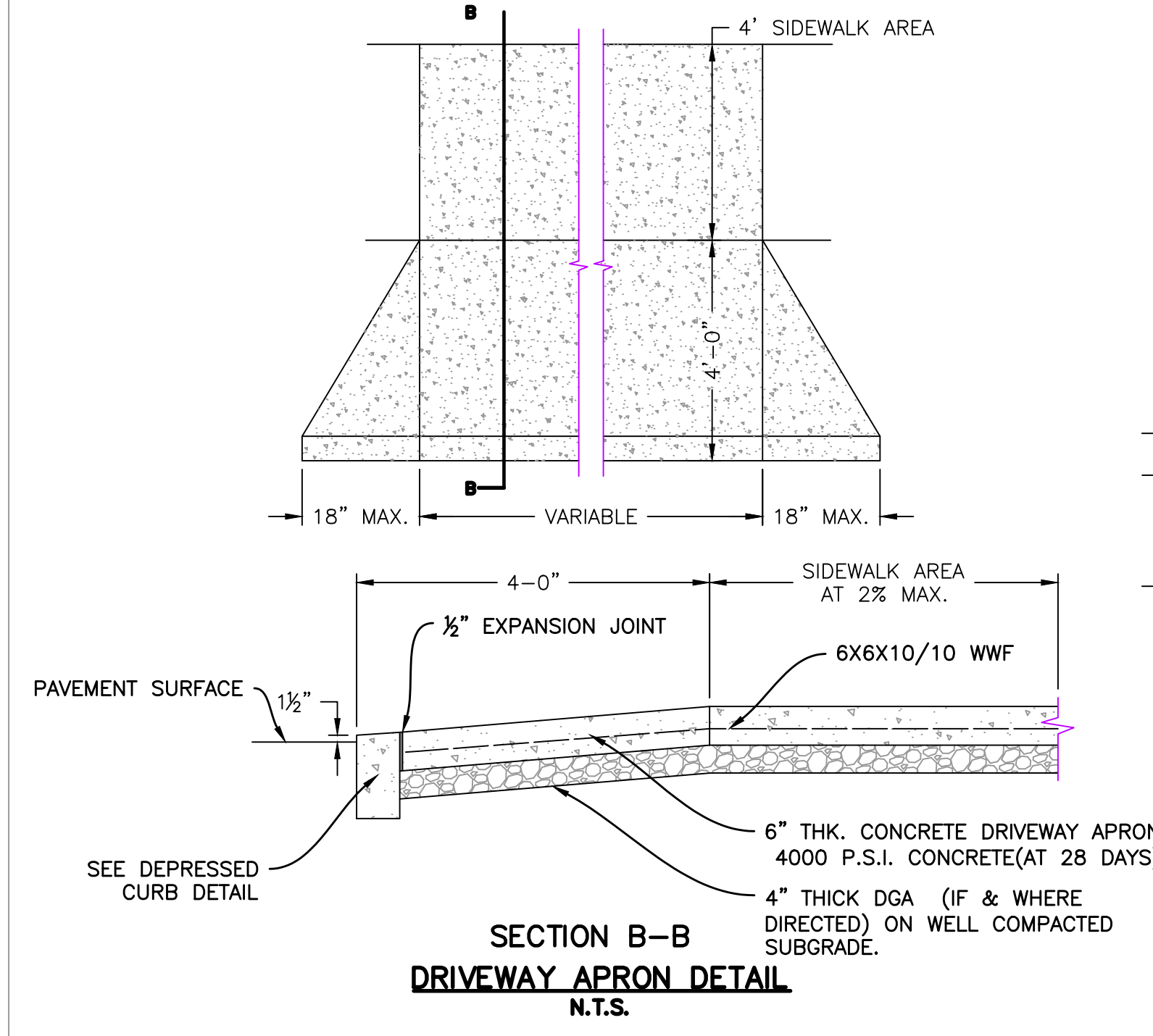
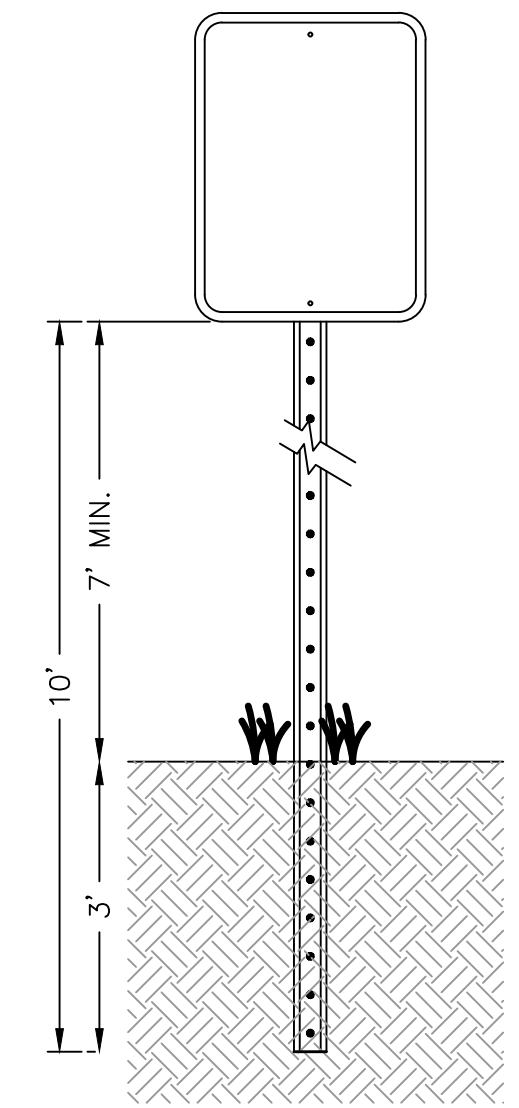
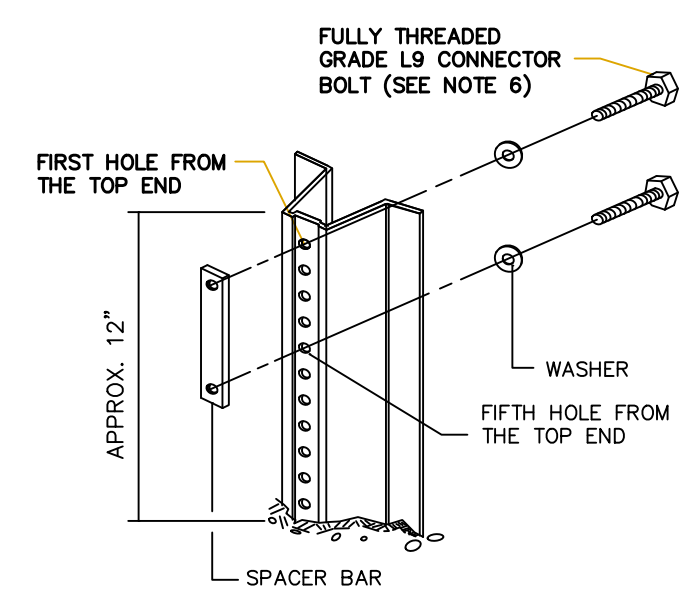
DOUBLE YELLOW THERMOPLASTIC DETAIL
N.T.S.

SINGLE WHITE THERMOPLASTIC DETAIL
N.T.S.

BASE REPAIR DETAIL
N.T.S.

CONCRETE GUTTER DETAIL
N.T.S.

CONCRETE SIDEWALK DETAIL
N.T.S.

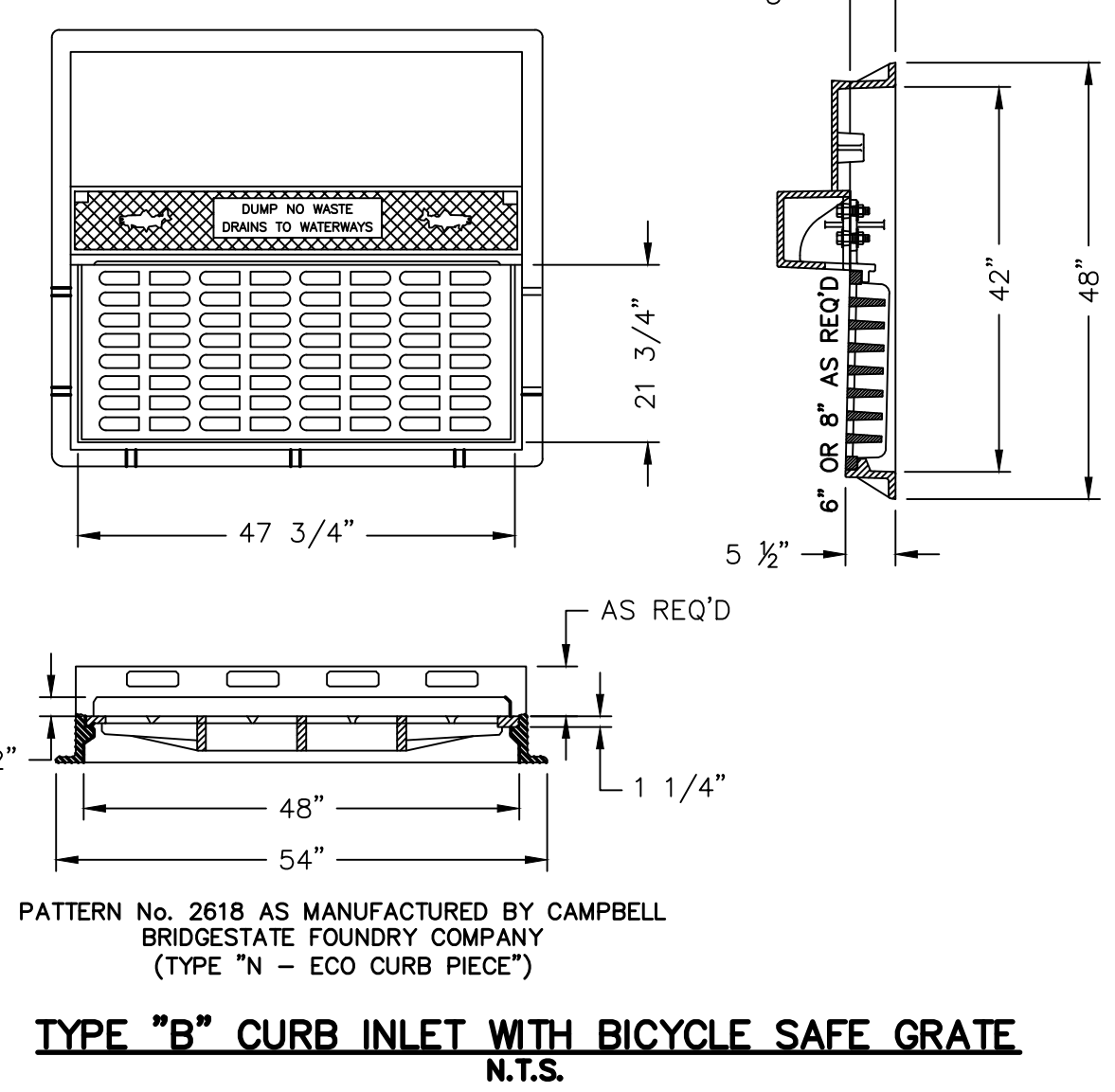
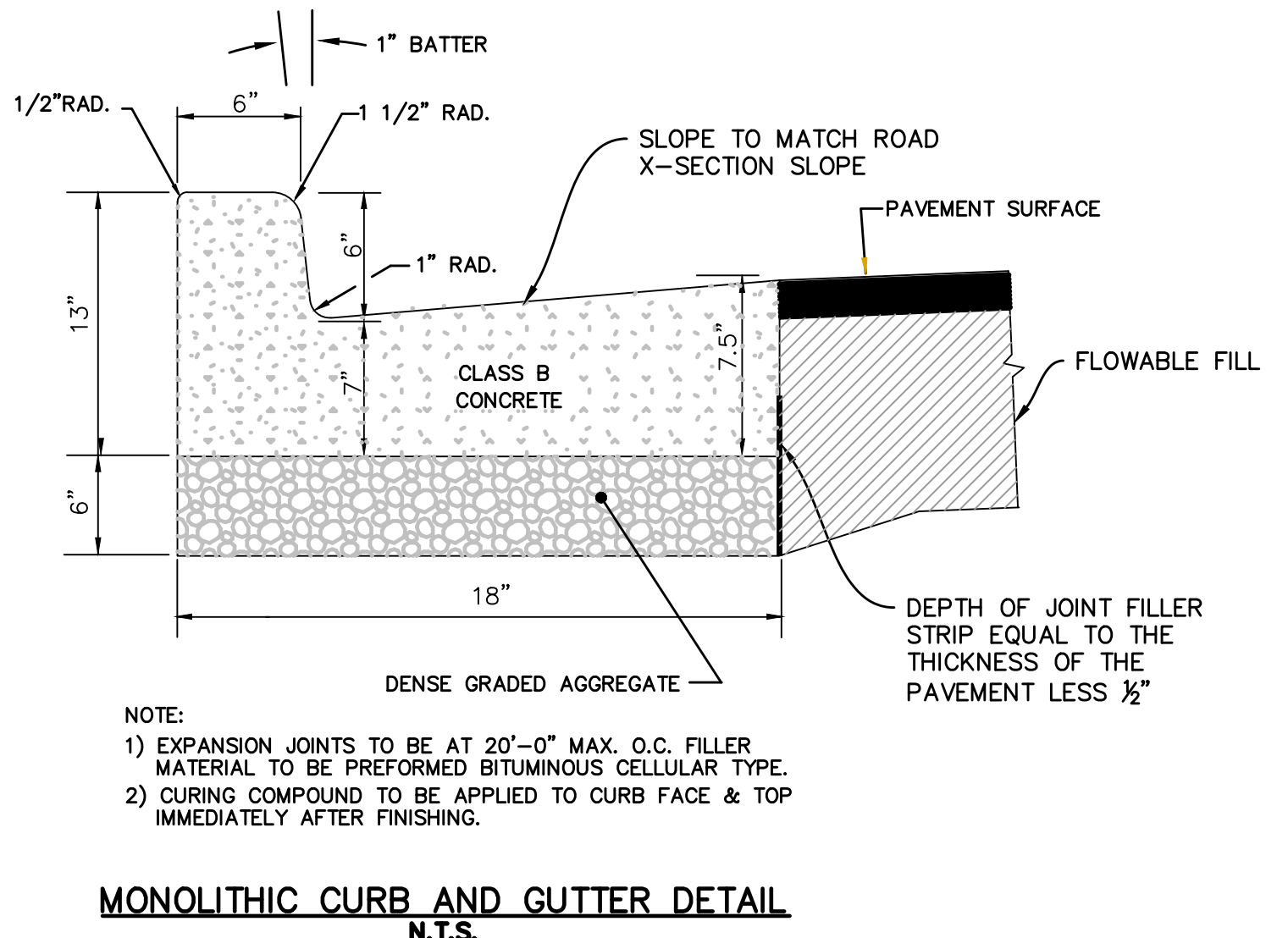
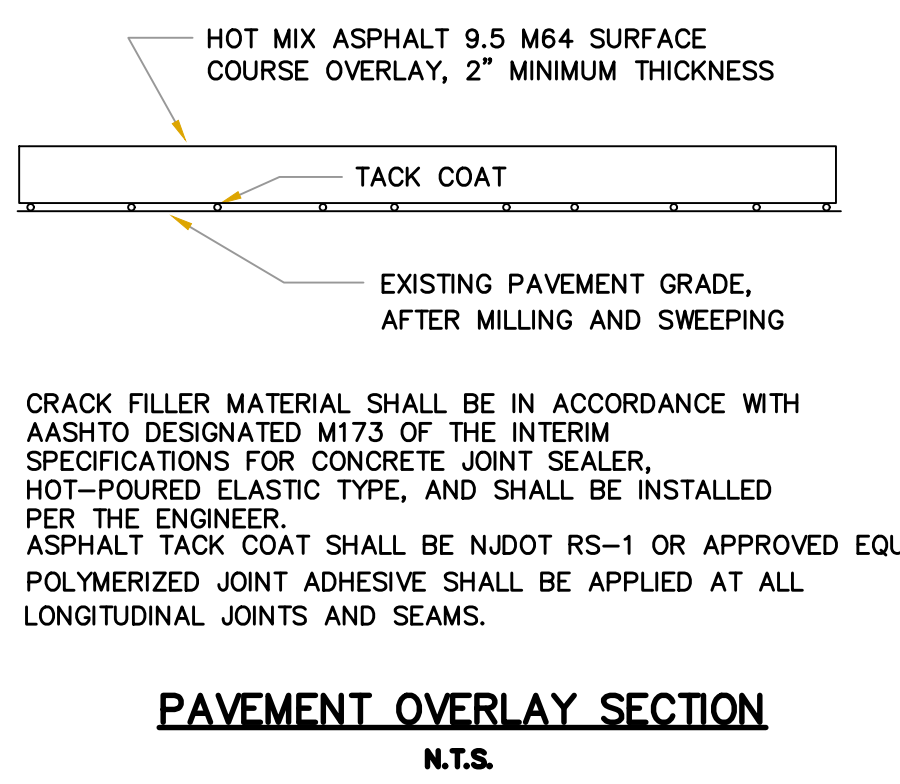
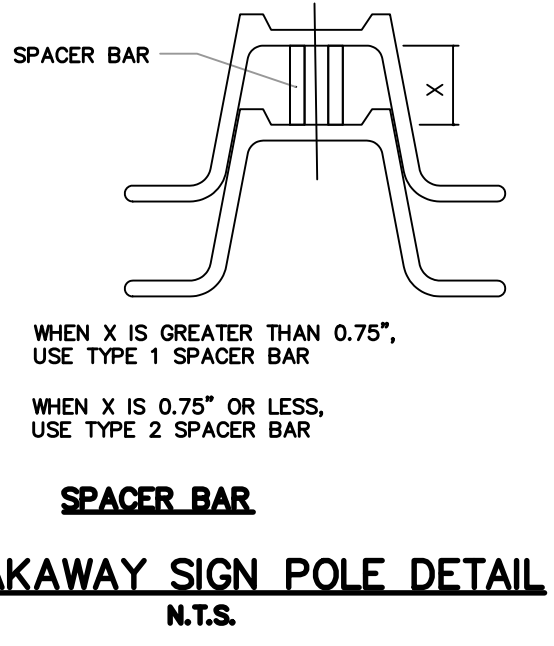
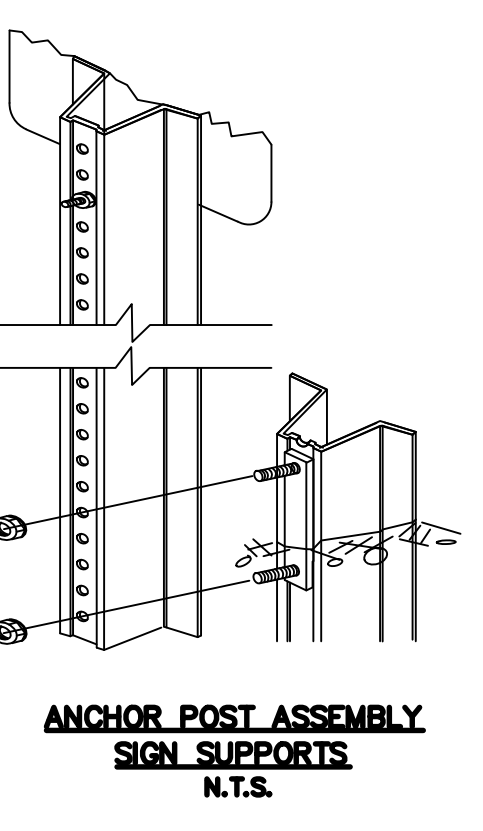


SECTION B-B DRIVEWAY APRON DETAIL
N.T.S.

DEPRESSED CURB DETAIL
N.T.S.

CONCRETE SWALE DETAIL
N.T.S.

LOCAL CONCRETE CURB DETAIL
N.T.S.

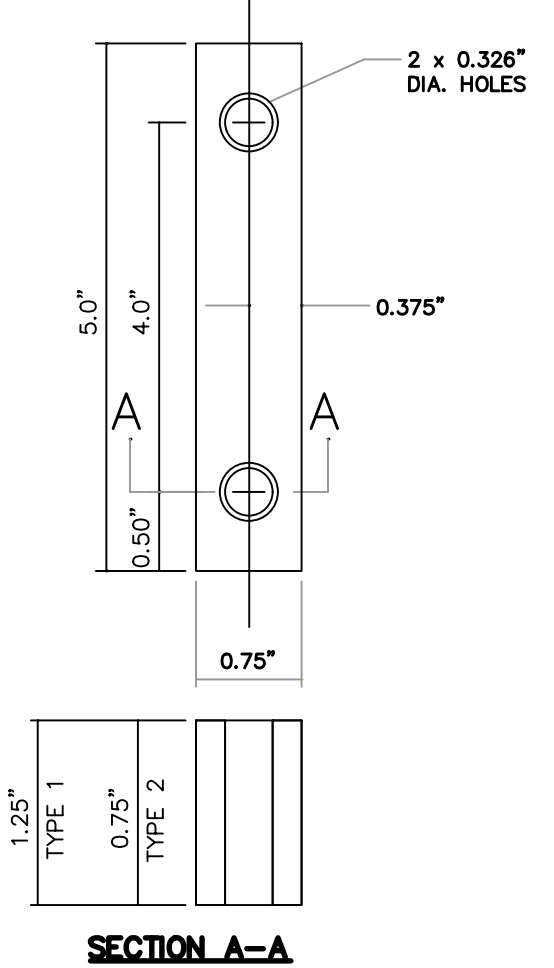


CRACK FILLER MATERIAL SHALL BE IN ACCORDANCE WITH AASHTO DESIGNATED M173 OF THE INTERIM SPECIFICATIONS FOR CONCRETE JOINT SEALER, HOT-POURED ELASTIC TYPE, AND SHALL BE INSTALLED PER THE ENGINEER. ASPHALT TACK COAT SHALL BE NJDOT RS-1 OR APPROVED EQUAL. POLYMERIZED JOINT ADHESIVE SHALL BE APPLIED AT ALL LONGITUDINAL JOINTS AND SEAMS.

NOTE:
1) EXPANSION JOINTS TO BE AT 20'-0" MAX. O.C. FILLER MATERIAL TO BE PERFORMED BITUMINOUS CELLULAR TYPE.
2) CURING COMPOUND TO BE APPLIED TO CURB FACE & TOP IMMEDIATELY AFTER FINISHING.

PATTERN No. 2618 AS MANUFACTURED BY CAMPBELL BRIDGECASTE FOUNDRY COMPANY (TYPE 'N' - ECO CURB PIECE')

- NOTES:
1. DRIVE ANCHOR POST ASSEMBLY TO WITHIN APPROXIMATELY 12 INCHES ABOVE GROUND LEVEL. PLACE BOLT AND WASHER IN FIRST AND FIFTH HOLES FROM THE TOP END, AND SECURE BOLTS ONTO SPACER.
 2. DRIVE ANCHOR POST ASSEMBLY TO WITHIN A MAXIMUM OF 4 INCHES ABOVE GROUND LEVEL.
 3. DIG OUT AROUND BACK OF ANCHOR POST ASSEMBLY TO ALLOW ROOM FOR TOP POST TO BE ATTACHED.
 4. NEST TOP POST ASSEMBLY ONTO PROTRUDING ANCHOR POST ASSEMBLY BOLTS, THROUGH THE FIRST AND FIFTH HOLES FROM THE BOTTOM OF THE TOP POST.
 5. PLACE AND TIGHTEN A SELF-LOCKING FLANGE NUT ON EACH BOLT. WHEN INSTALLATION IS COMPLETE, TOP OF GROUND POST SHALL NOT EXCEED 4 INCHES ABOVE GROUND LEVEL.
 6. SIZE OF CONNECTOR BOLT FOR TYPE 1, 1/2" x 1/2" SIZE OF CONNECTOR BOLT FOR TYPE 2, 3/8" x 2"
 7. THE CONNECTOR BOLTS SHALL BE FULLY THREADED. EACH CONNECTOR BOLT AND NUT SHALL BE CLEARLY STAMPED WITH MANUFACTURER'S IDENTIFYING MARK.

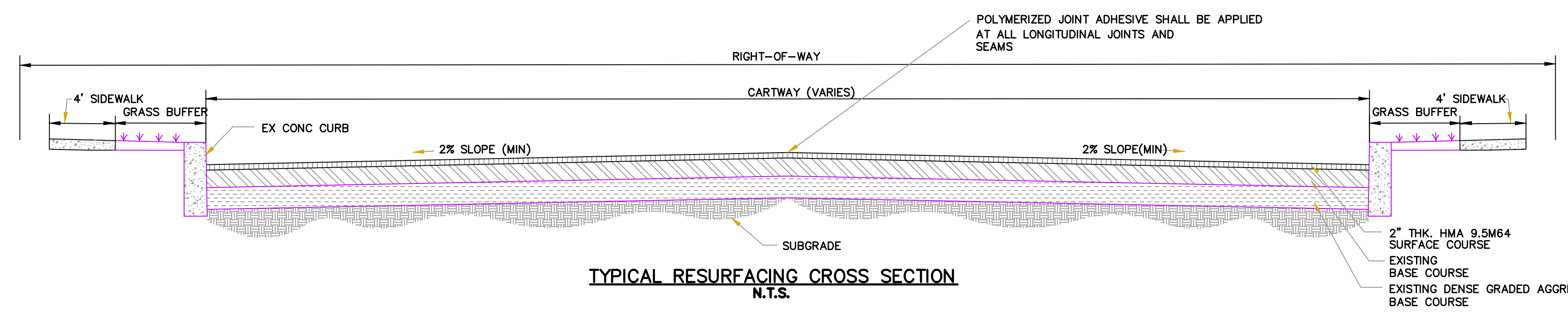


BREAKAWAY SIGN POLE DETAIL
N.T.S.

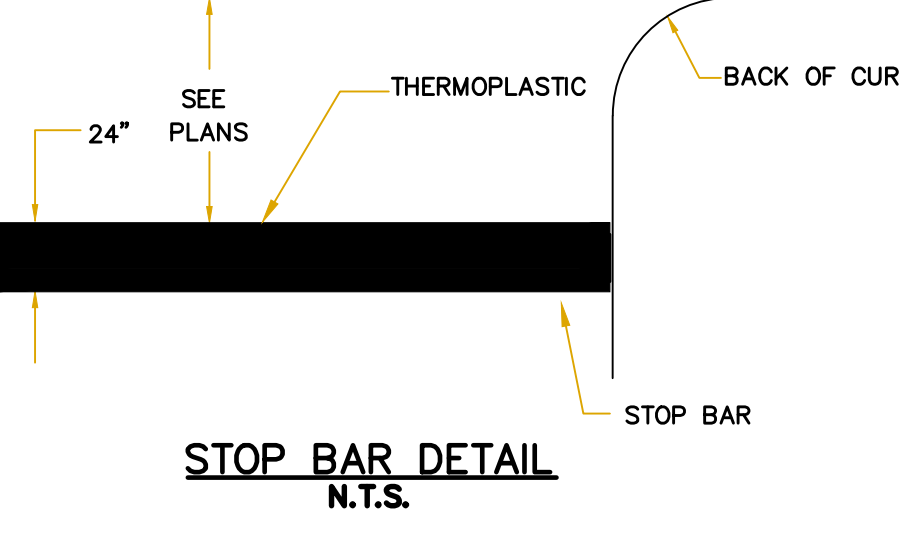
PAVEMENT OVERLAY SECTION
N.T.S.

MONOLITHIC CURB AND GUTTER DETAIL
N.T.S.

TYPE "B" CURB INLET WITH BICYCLE SAFE GRATE
N.T.S.



TYPICAL RESURFACING CROSS SECTION
N.T.S.



STOP BAR DETAIL
N.T.S.

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NJ COA NO. 0429033300

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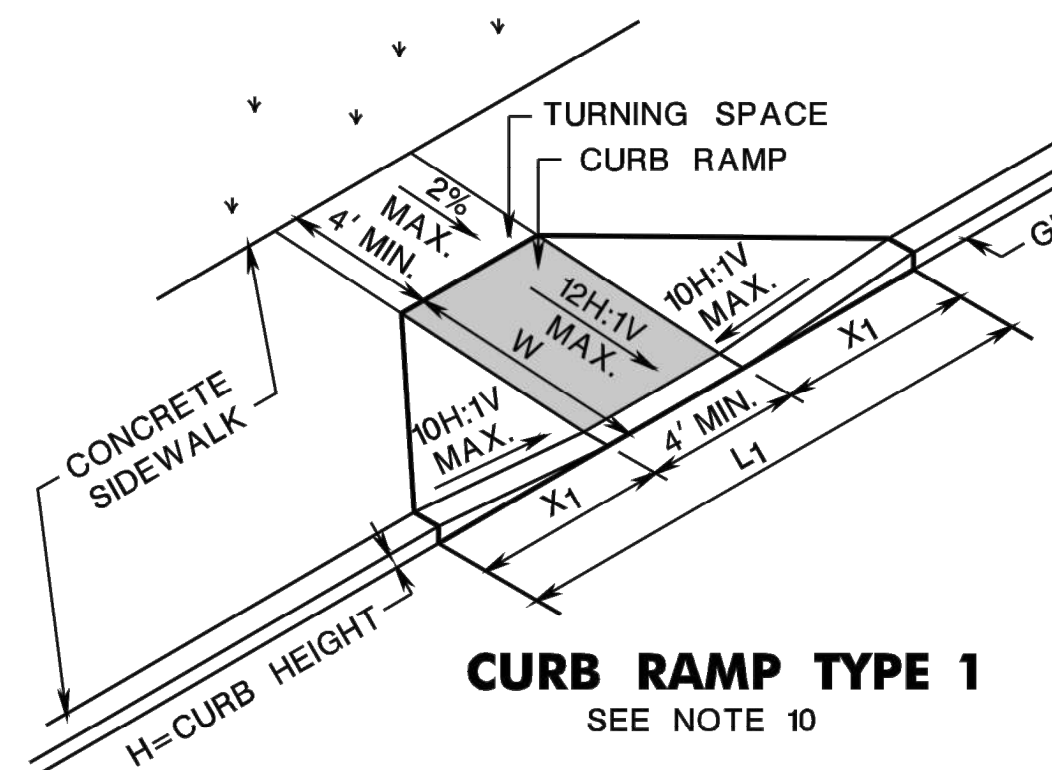
NJDOT FY2021 MUNICIPAL AID PROGRAM
WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY
CONSTRUCTION DETAILS
WILLINGBORO TOWNSHIP
1 REV DR MARTIN LUTHER KING JR RD
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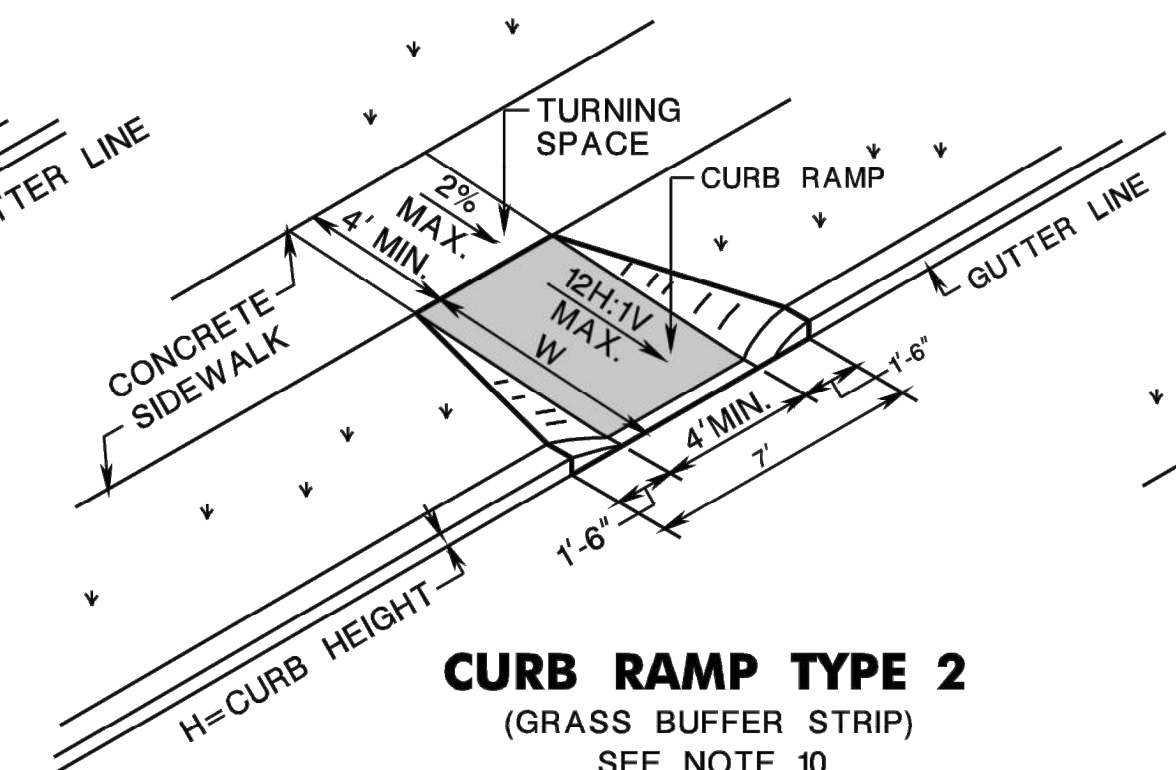
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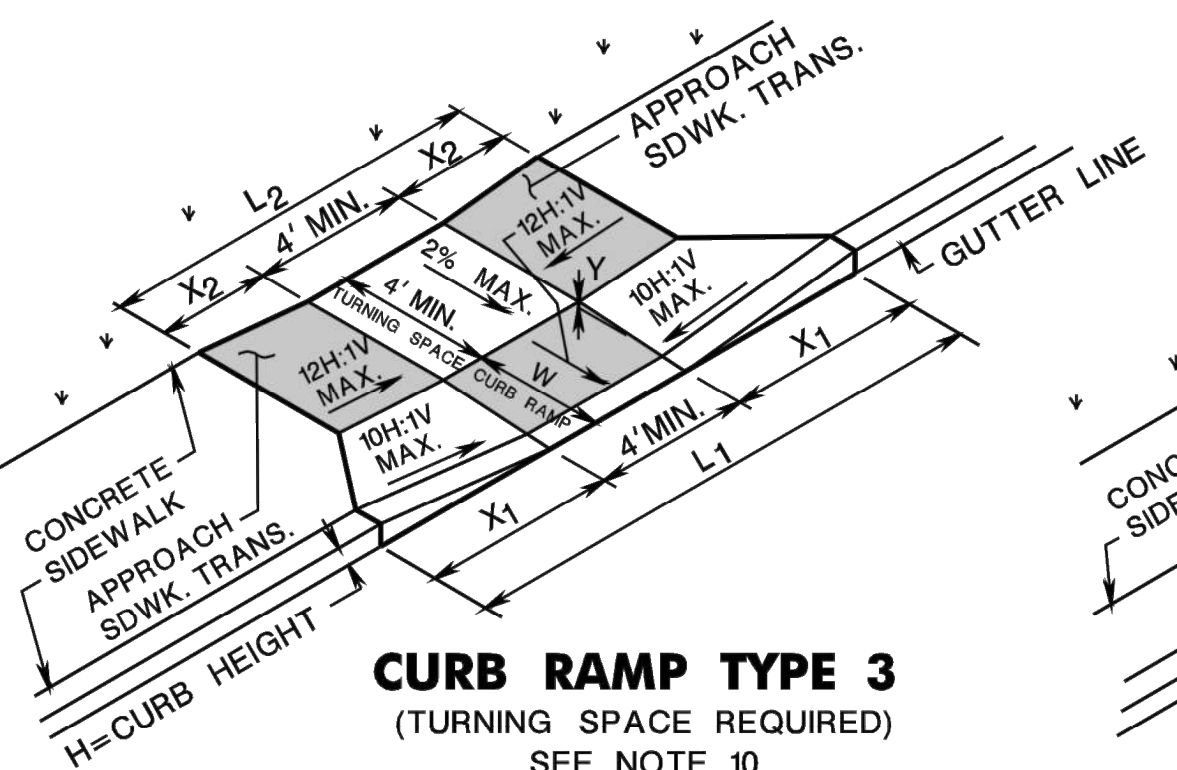
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SHEET 6 OF 10



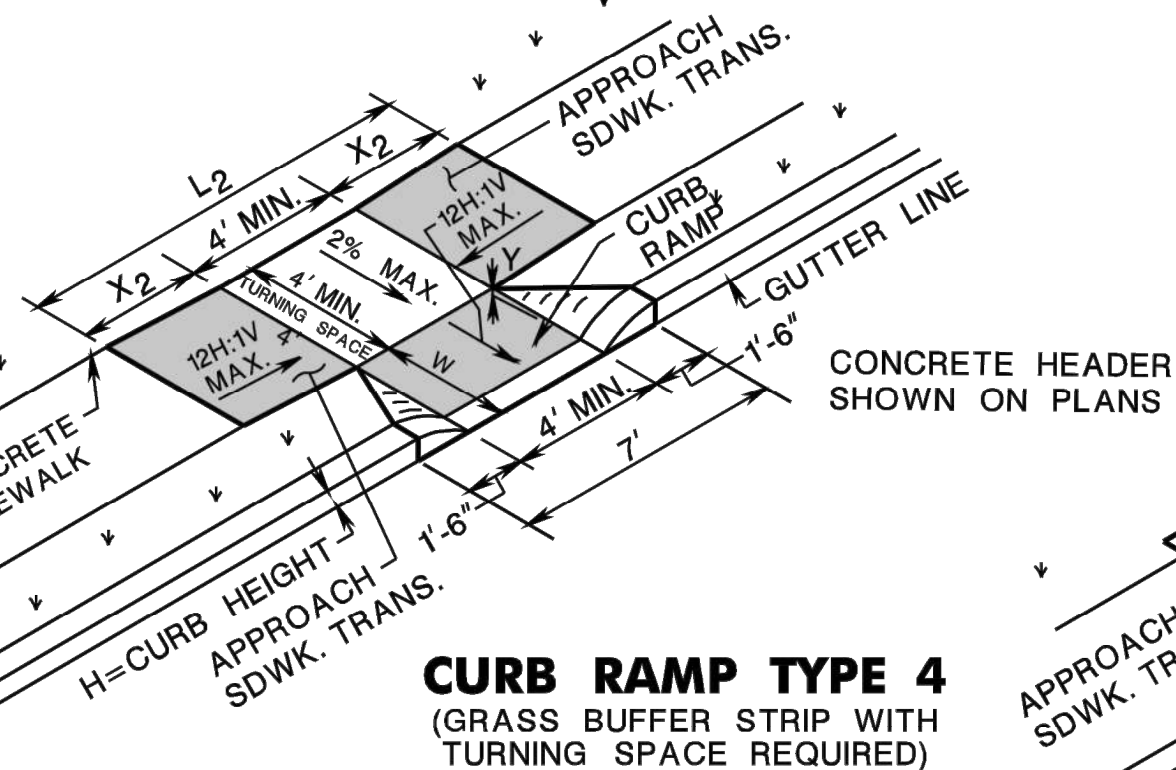
CURB RAMP TYPE 1
SEE NOTE 10



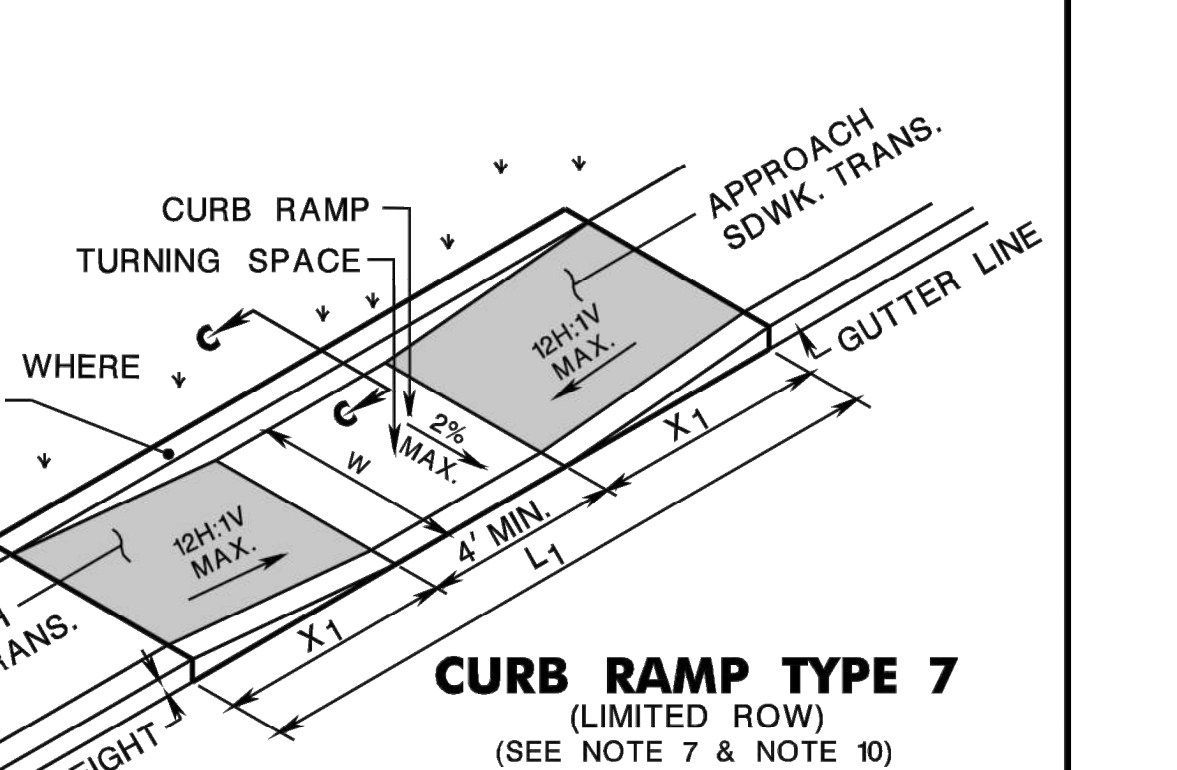
CURB RAMP TYPE 2
(GRASS BUFFER STRIP)
SEE NOTE 10



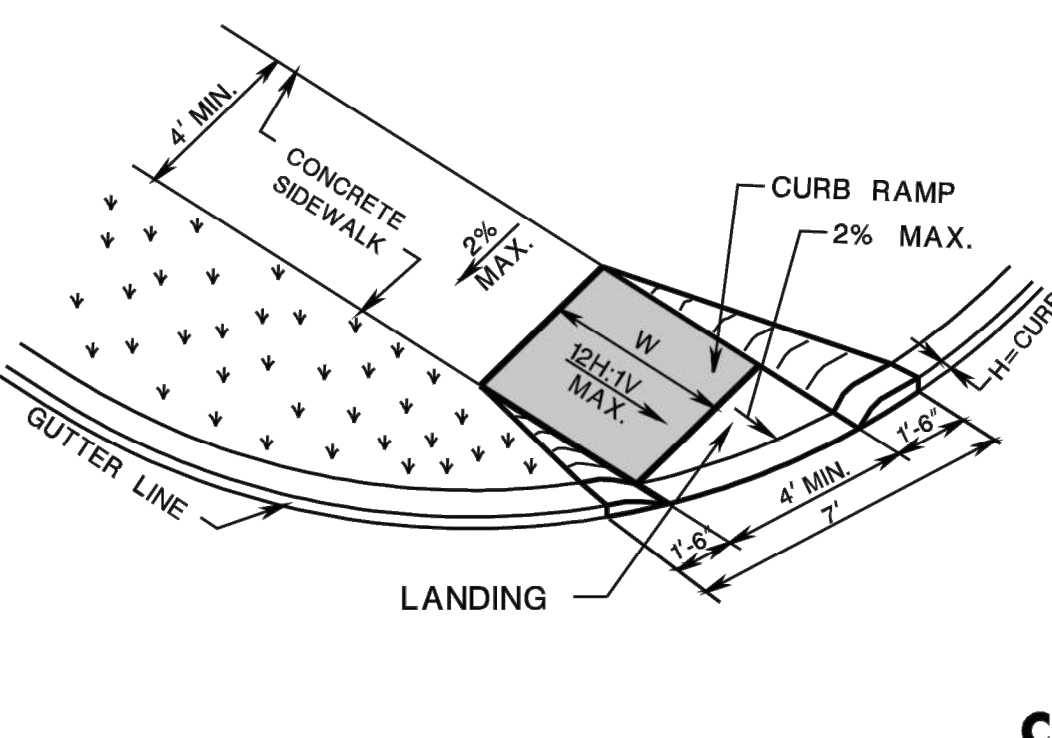
CURB RAMP TYPE 3
(TURNING SPACE REQUIRED)
SEE NOTE 10



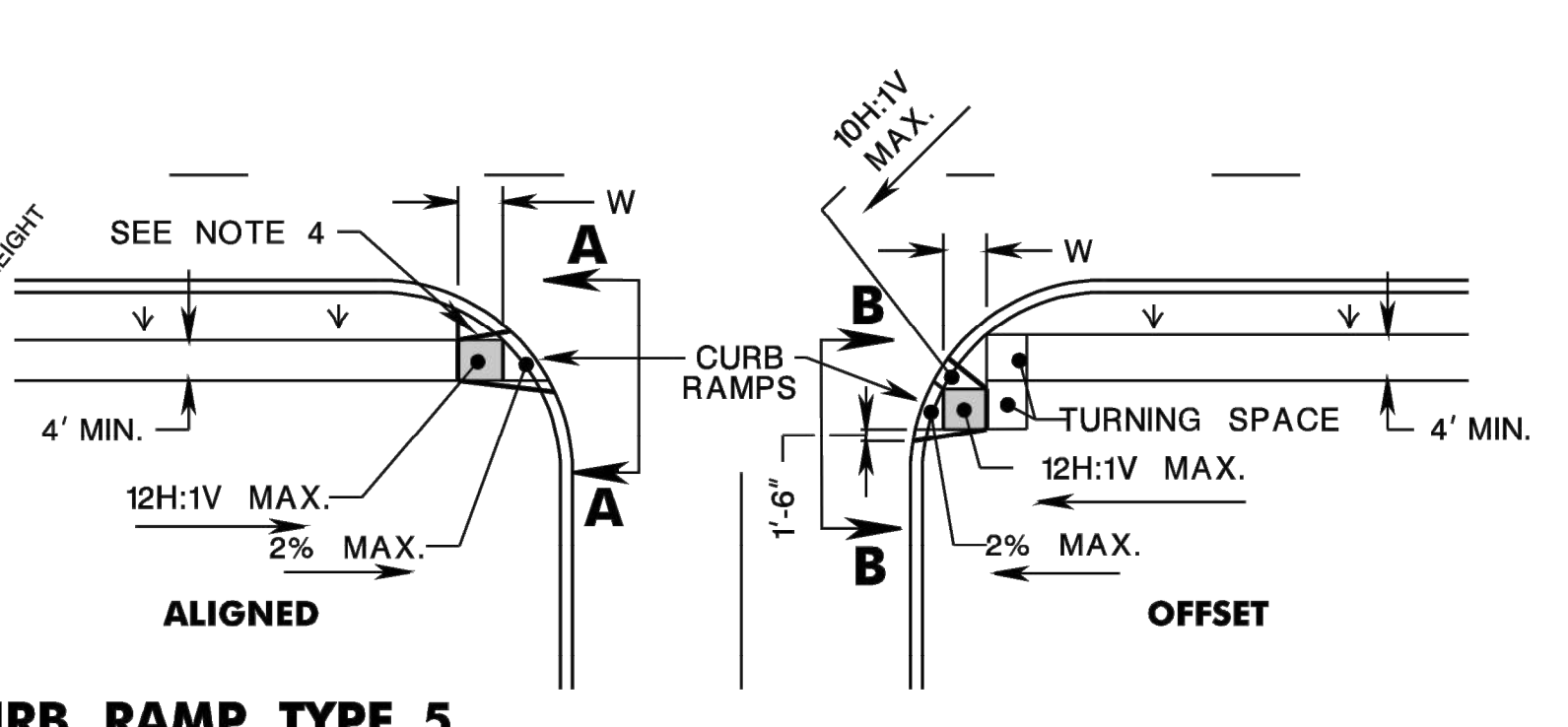
CURB RAMP TYPE 4
(GRASS BUFFER STRIP WITH
TURNING SPACE REQUIRED)
SEE NOTE 10



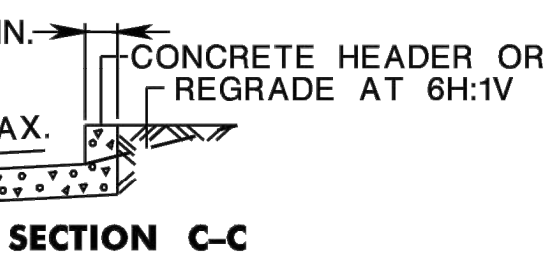
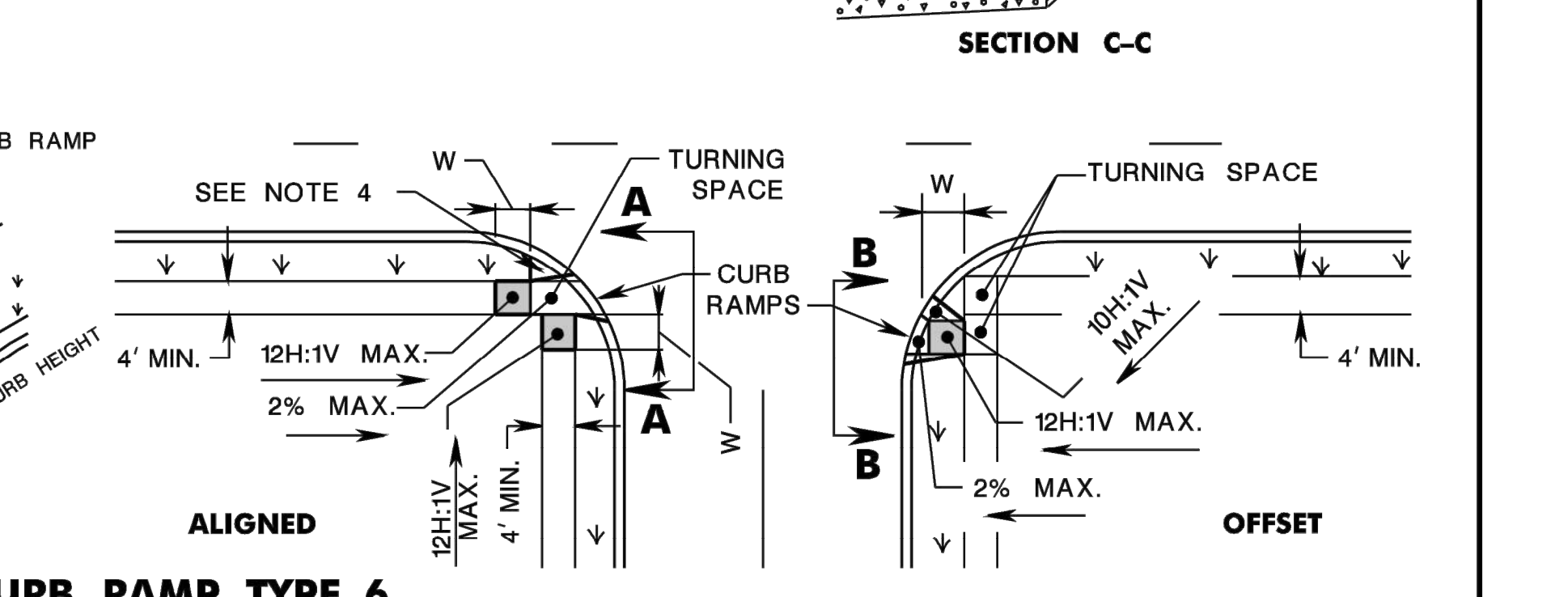
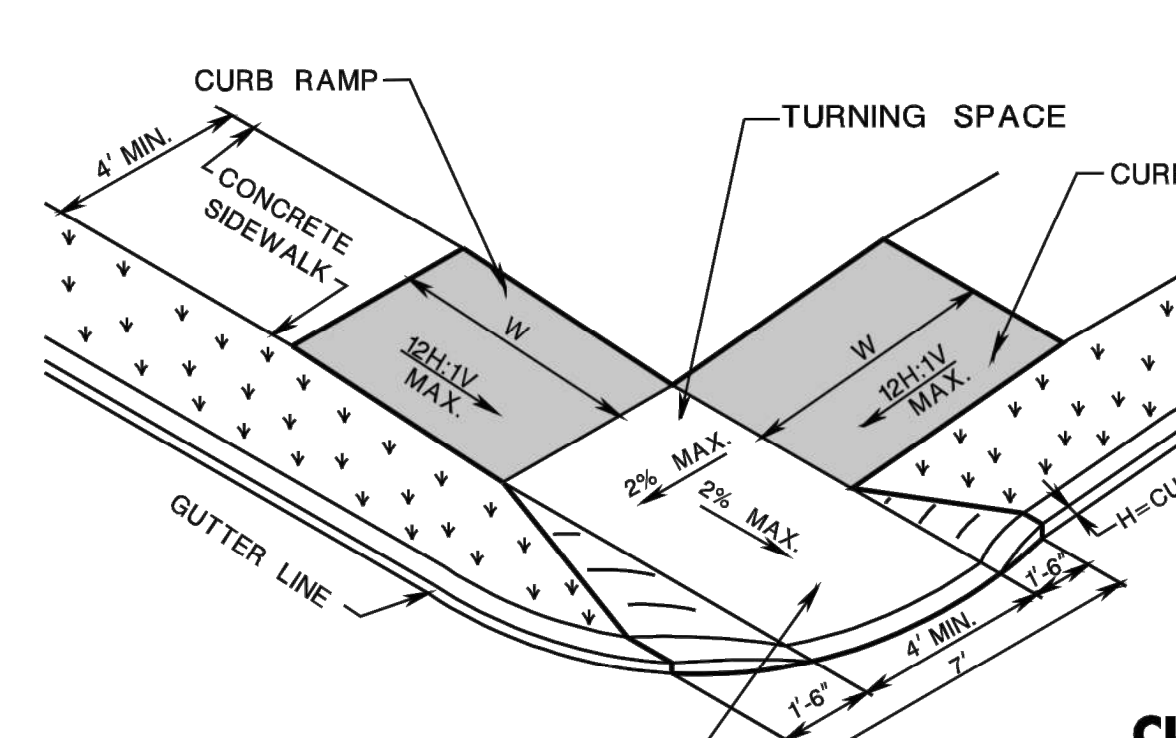
CURB RAMP TYPE 7
(LIMITED ROW)
(SEE NOTE 7 & NOTE 10)



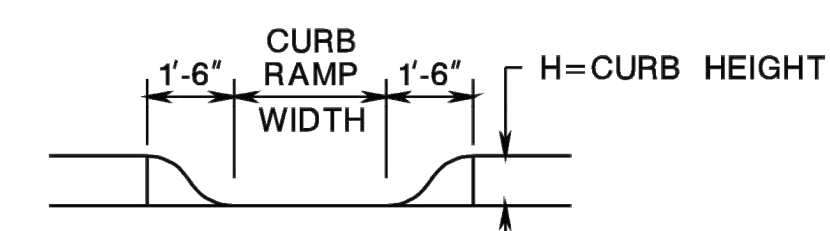
CURB RAMP TYPE 5



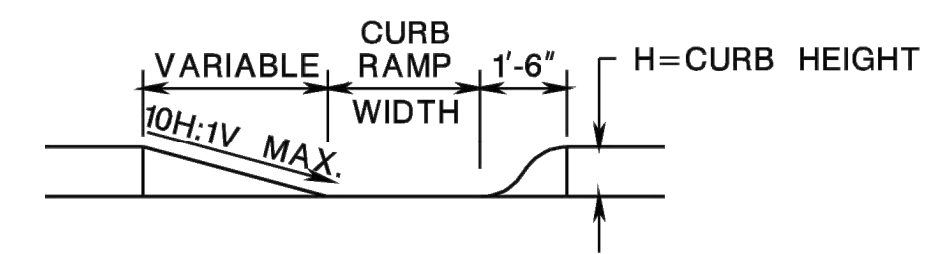
CURB RAMP TYPE 6



SECTION C-C

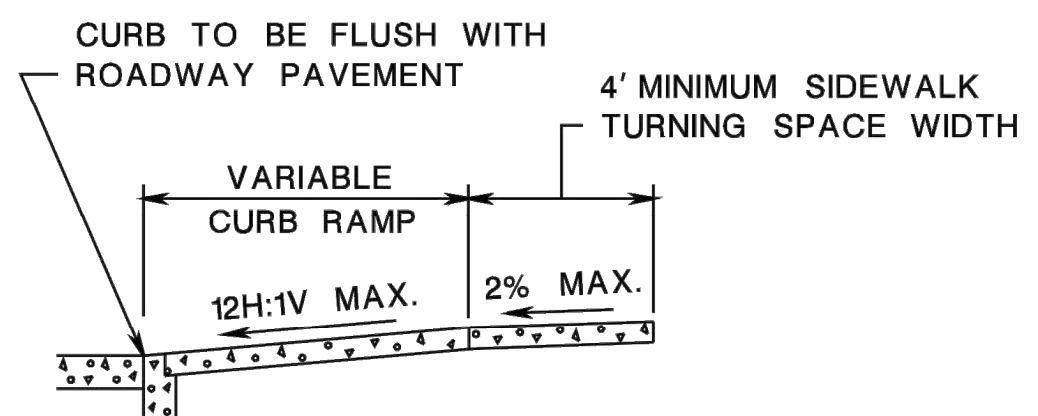


SECTION A-A

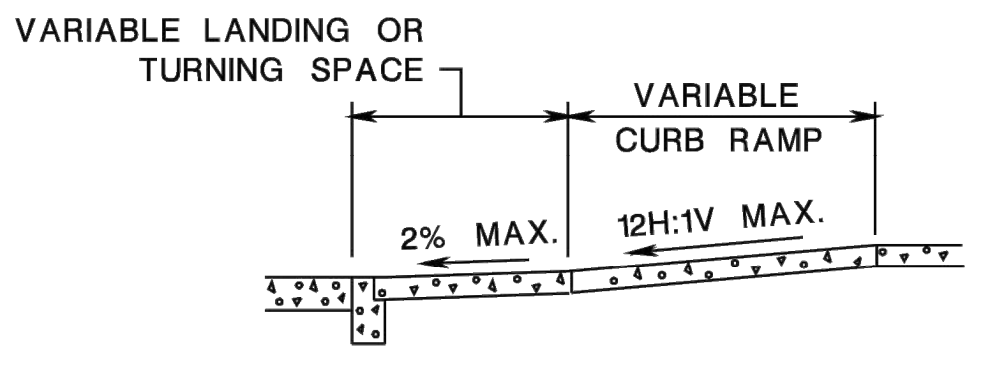


SECTION B-B

NOTE:
CURB RAMP OPENING TO BE FLUSH WITH ROADWAY PAVEMENT (CURB RAMP TYPES 5 & 6).

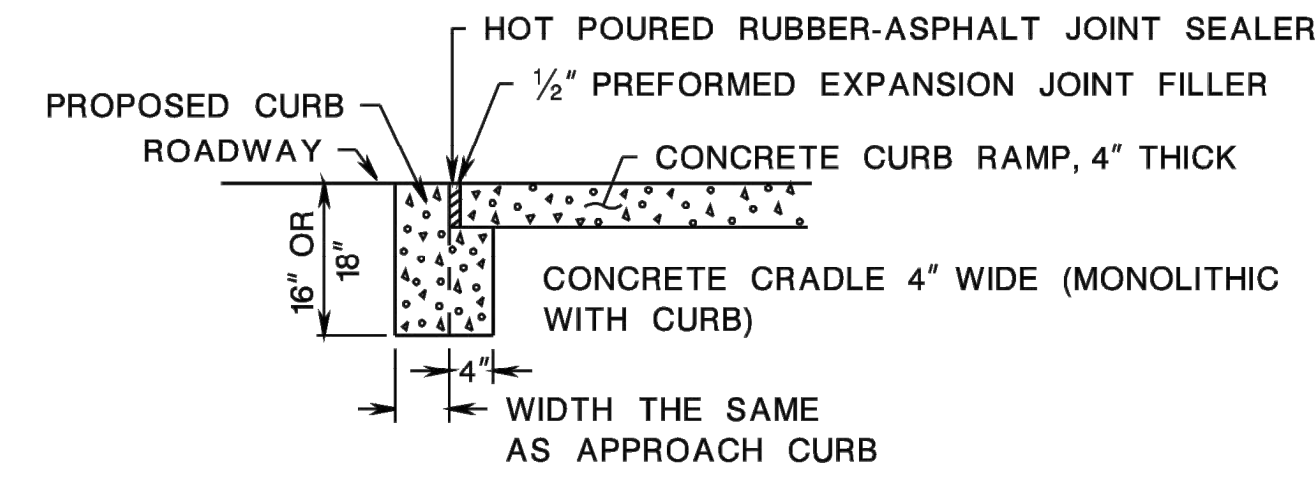


SECTION THROUGH CURB RAMPS 1 THROUGH 4



SECTION THROUGH CURB RAMPS 5 AND 6

- NOTES:**
- KEEP TURNING SPACE, APPROACH SIDEWALK TRANSITIONS, AND CURB RAMP CLEAR OF OBSTRUCTIONS THAT PROTRUDE ABOVE THE SURFACE.
 - FOR DIMENSIONS SEE CD-606-3 AND CD-606-4
 - CURB (DROPPED CURB) GUTTERLINE TO BE FLUSH WITH ROADWAY PAVEMENT THE ENTIRE WIDTH OF THE RAMP (4 FEET MIN.) AT ALL CURB RAMPS.
 - FOR CURB RAMP TYPES 5 AND 6, IF A GRASS BUFFER DOES NOT EXIST, SLOPE CURB TO EQUAL SLOPE OF ADJACENT CURB RAMP.
 - SIDEWALK AND CURB RAMP WITHIN AREA ENCLOSED BY HEAVY LINES INDICATES THE PAY LIMIT FOR CONCRETE SIDEWALK OF THE APPROPRIATE ADJACENT THICKNESS.
 - CURB AND HEADER WITHIN AREA ENCLOSED BY HEAVY LINES INDICATES THE PAY LIMIT FOR VERTICAL CURB OR SLOPING CURB OF THE APPROPRIATE ADJACENT SIZE AND KIND.
 - WHERE THE DISTANCE FROM THE GUTTER LINE TO THE OUTSIDE EDGE OF SIDEWALK IS 6 FEET OR LESS, USE CURB RAMP TYPE 7, INSTEAD OF CURB RAMP TYPE 1 THROUGH 4.
 - CROSSWALKS AND STOP LINES MAY BE MARKED OR UNMARKED. SEE PLANS.
 - THE 12H:1V MAX SLOPE IS THE RUNNING SLOPE FOR CURB RAMPS, BUT ONLY THE 12H:1V SLOPE MEASURED AS X₂ IS THE RUNNING SLOPE FOR TYPE 3 AND TYPE 4 CURB RAMPS. ENSURE THE RUNNING SLOPE OF CURB RAMPS DOES NOT REQUIRE ITS LENGTH TO EXCEED 15 FEET. THE RUNNING SLOPE MAY EXCEED THE 12H:1V MAX SLOPE SO AS NOT TO EXCEED THE 15 FEET MAXIMUM LENGTH.
 - CURB RAMP TYPE 1 THROUGH 7 ARE NORMALLY PLACED ON THE RADIUS RETURN AT THE INTERSECTION AND ON A TANGENT SECTION AS DRAWN.



DROPPED CURB AND CRADLE

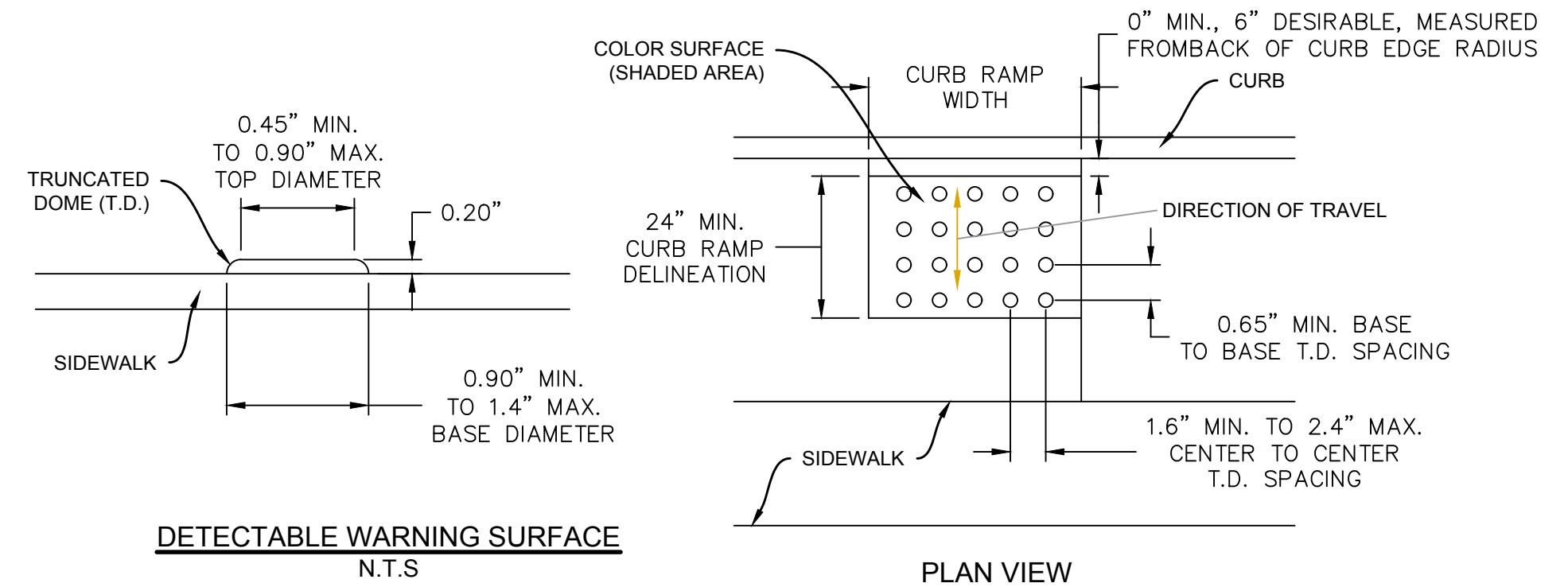
CONCRETE SIDEWALK
(PUBLIC SIDEWALK CURB RAMP)
N.T.S.

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

CURB RAMPS
CD-606-1.1

52
164



DETECTABLE WARNING SURFACE
N.T.S.

PLAN VIEW

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NJ COA NO. 0429033300

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WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

CONSTRUCTION DETAILS

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DRAWN BY: SMR
APPROVED BY: HJD

CM6002
SHEET 7 OF 10

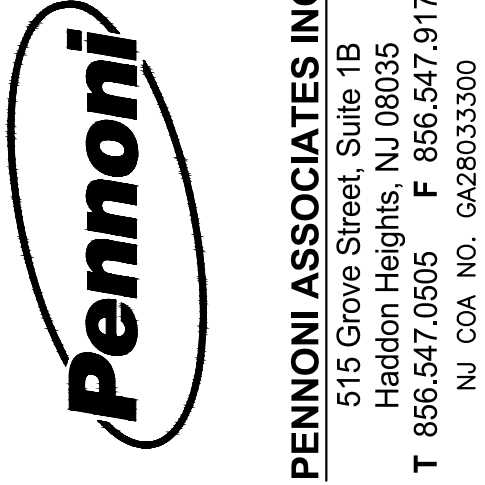
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LEGEND

-  BREAKAWAY BARRICADES
-  BREAKAWAY BARRICADES WITH SIGN
-  CONSTRUCTION SIGNS
-  DRUMS
-  CONE
-  CONSTRUCTION BARRIER CURB (TYPE SPECIFIED)
-  DIRECTION OF TRAFFIC FLOW
-  TRAFFIC DIRECTOR, FLAGGER
-  TRAILER MOUNTED MOUNTED ARROW BOARD SHOWING CAUTION MODE
-  ILLUMINATED FLASHING ARROW MOUNTED ON TOWING VEHICLE SHOWING ARROW PATTERN (LEFT, RIGHT, BOTH)
-  TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING CAUTION MODE
-  TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING ARROW PATTERN (LEFT, RIGHT, BOTH)
-  TEMPORARY CRASH CUSHION, INERTIAL BARRIER SYSTEM
-  TEMPORARY CRASH CUSHION, (ALL OTHER APPROVED)
-  BUFFER ZONE
-  WORK AREA
-  PAINT STRIPING TRUCK OR OTHER OPERATING VEHICLE

GENERAL NOTES:

1. ADVANCE WARNING SIGNS DISTANCES AND TAPER LENGTHS MAY BE EXTENDED, AT DIRECTION OF THE DEPARTMENT, TO ADJUST FOR REDUCED VISIBILITY DUE TO HORIZONTAL AND VERTICAL CURVATURE OF THE ROADWAY.
2. THE APPROXIMATE LOCATIONS OF THE ILLUMINATED FLASHING ARROW BOARDS ARE SHOWN ON THE TRAFFIC CONTROL PLANS. THESE LOCATIONS MAY BE MODIFIED AS APPROVED BY RE TO ADJUST FOR VISIBILITY DUE TO HORIZONTAL OR VERTICAL CURVATURE OF THE ROADWAY OR TO POSITION AT A SAFER LOCATION. ILLUMINATED FLASHING ARROW BOARDS ARE TO BE USED FOR TEMPORARY LANE CLOSINGS AND AT LOCATIONS SHOWN ON THE TRAFFIC CONTROL PLANS.
3. PRIOR TO ANY ROAD CONSTRUCTION, TRAFFIC CONTROL SIGNS AND DEVICES ARE TO BE IN PLACE.
4. RAMPS AND/OR SIDE STREETS ENTERING THE ROADWAY AFTER THE FIRST ADVANCE WARNING SIGN ARE TO BE PROVIDED WITH AT LEAST ONE W20-F SIGN (ROAD WORK AHEAD) AS A MINIMUM.
5. ALL EXISTING ROAD SIGNS, PAVEMENT MARKINGS, AND /OR PLOWABLE PAVEMENT REFLECTORS WHICH CONFLICT WITH THE PROPOSED TRAFFIC CONTROL PLAN ARE TO BE COVERED, REMOVED, OR RELOCATED AS DIRECTED BY THE RE.
6. CONFLICTING OR NON-OPERATING SIGNAL INDICATIONS ON EITHER THE EXISTING, TEMPORARY, OR PROPOSED TRAFFIC SIGNAL SYSTEMS ARE TO BE BAGGED OR COVERED.
7. MAINTENANCE AND PROTECTION OF TRAFFIC TO BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES - PART VI "STANDARDS AND GUIDES FOR TRAFFIC CONTROL FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, UTILITY, AND INCIDENT MANAGEMENT OPERATIONS", UNLESS OTHERWISE NOTED IN THE PLANS AND SPECIFICATIONS.
8. CONSTRUCTION SIGN W99-2 (GIVE US A BRAKE) TO BE LOCATED 200 FEET IN ADVANCE OF PROJECT LIMITS.
9. A W1-6 (ARROW) SIGN MOUNTED ON A BREAKAWAY BARRICADE AND CENTERED ON THE CLOSED WIDTH TO BE LOCATED 100 FEET BEYOND EACH INTERSECTION OR MAIN ACCESS POINT WITHIN THE AREA OF A LANE OR SHOULDER CLOSURE.
10. CONSTRUCTION SIGNS R11-4 (ROAD CLOSED THRU TRAFFIC) TO BE PLACED AT THE INTERSECTING STREETS WHICH ARE CLOSED TO TRAFFIC BECAUSE OF CONSTRUCTION.
11. CONSTRUCTION SIGNS W8-9A (SYMBOL FOR UNEVEN PAVEMENT) AND W8-14A (GROOVED PAVEMENT) TO BE USED WHEN SUCH PAVEMENT CONDITIONS EXIST.
12. MOVING WORK AREAS IN A LANE CLOSURE REQUIRE A TRAILER MOUNTED ILLUMINATED FLASHING ARROW TO REMAIN AT THE END OF THE TAPER, THE TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION THAT IS TO MOVE WITH THE WORK AREAS TO KEEP A 70 FEET MIN. AND 150 FEET MAX. BUFFER IN ADVANCE OF EACH WORK AREA.
13. THE CONTRACTOR TO SUBMIT A PLAN FOR THE SAFE ACCESS OF CONSTRUCTION VEHICLES THROUGHOUT THE WORK SITE WHERE SPACE CONSTRAINTS PREVENT THE USE OF LANE CLOSURES. THE PLAN TO BE SUBMITTED TO THE RE AS SPECIFIED IN THE SPECIFICATIONS.
14. BACKFILL ALL EXCAVATED AREAS WITHIN OR ADJACENT TO THE ROADWAY AND PLACE ON AT LEAST 6H:1V SLOPE BEFORE THE END OF EACH WORK DAY. OTHER EXCAVATED AREA WITHIN THE CLEAR ZONE ARE TO BE BACKFILLED.
15. WHERE REQUIRED, THE CONTRACTOR IS TO MAKE PROVISIONS FOR MAINTAINING PEDESTRIAN CROSSING LOCATIONS AND TYPE AS DIRECTED BY THE RE.
16. BITUMINOUS CONCRETE PLACED DURING THE VARIOUS CONSTRUCTION STAGES TO BE TRANSITIONED ON A MINIMUM 20H:1V SLOPE TO MEET THE ADJACENT EXISTING GRADE AT THE LONGITUDINAL AND TRANSVERSE LIMITS OF THE STAGE CONSTRUCTION AREAS UNLESS OTHERWISE NOTED ON THE STAGE CONSTRUCTION PLANS.
17. THE PLACEMENT AND /OR RELOCATION OF CONSTRUCTION BARRIER CURB TO BE DONE DURING ALLOWABLE LANE CLOSURE HOURS.
18. CONSTRUCTION ZONE SPEED LIMIT WILL BE DETERMINED BY THE BUREAU OF TRAFFIC ENGINEERING, REGIONAL TRAFFIC ENGINEER - WORK ZONE, AT THE TIME OF OR DURING CONSTRUCTION, AS REQUESTED BY THE RE.
19. THE SPEED LIMIT, R2-1 (BLACK ON WHITE) WITH ADDED WORK ZONE PLATE (BLACK ON ORANGE) SIGNS TO BE LOCATED THROUGH WORK AREAS AS DIRECTED BY THE BUREAU OF TRAFFIC ENGINEERING, REGIONAL TRAFFIC ENGINEER - WORK ZONE.
20. THE REDUCED SPEED AHEAD SIGN, W3-5(S) (BLACK ON ORANGE) TO BE LOCATED IN ADVANCE OF SPEED LIMIT R2-1 SIGNS WHICH REDUCE THE NORMAL POSTED SPEED LIMIT THROUGH THE CONSTRUCTION ZONE.
21. TRAFFIC FINES DOUBLED IN WORK AREA R(NJ)5-17(S), 4 FEET BY 2.5 FEET SIGN TO BE LOCATED 500 FEET AFTER THE FIRST ADVANCE WARNING SIGN, (W20 SERIES) AT EACH WORK AREA LOCATED WITHIN URBAN AREAS. THIS SIGN TO ALSO BE USED ON PROJECTS REQUIRING MOVING OPERATIONS IN WHICH CASE THE SIGN IS TO BE MOUNTED ON A SLOW MOVING CONSTRUCTION VEHICLE.
22. DO NOT CONSTRUCT THE FINAL HMA SURFACE PAVEMENT UNTIL THE FINAL STAGE OF THE PROJECT UNLESS OTHERWISE DIRECTED BY THE RE OR INDICATED ON THE PLANS. SET MANHOLES AND INLETS TO FINISHED GRADE AND CONSTRUCT TEMPORARY PAVEMENT RAMPS AROUND THEM WITH A MINIMUM 20H:1V SLOPE IN ALL DIRECTIONS USING HOT MIX ASPHALT PAVEMENT. THIS TEMPORARY MATERIAL WILL BE REMOVED IMMEDIATELY PRIOR TO PLACING THE SURFACE COURSE.
23. PLACE TRAFFIC CONTROL DEVICES FOR LANE CLOSURES INCLUDING SIGNS, CONES, BARRICADES, ETC. AS SHOWN ON PLANS. NO SIGNS ARE TO BE PLACED WITHOUT ACTUAL LANE CLOSURES AND REMOVE IMMEDIATELY UPON REMOVAL OF THE CLOSURES.
24. CONES MAY BE SUBSTITUTED FOR DRUMS AND INSTALLED UPON THE APPROVAL OF THE RE.
25. TRAFFIC IMPACT NOTICES AND CHANGES
 - A. TERMS: WHEN THE FOLLOWING TERMS ARE USED, THE INTENT AND MEANING IS AS FOLLOWS:
 - i. IMPACTS TO NORMAL TRAFFIC FLOW - WORK THAT REQUIRES A PORTION OF THE PAVED ROADWAY BEING BLOCKED OR CLOSED WITH SAFETY DEVICES OR VEHICLES, INCLUDING, BUT NOT LIMITED TO, FULL OR PARTIAL LANE CLOSURES, FULL OR PARTIAL RAMP CLOSURES, SHOULDER CLOSURES, MOVING OPERATIONS SUCH AS TRAFFIC STRIPING OR SWEEPING, LANE SHIFTS, OR ALTERNATING TRAFFIC. THIS APPLIES EVEN WHEN DETOURS ARE PROVIDED.
 - ii. TEMPORARY LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH IS ROUTINELY SET UP AND REMOVED ON A DAILY BASIS.
 - iii. PERMANENT LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH REMAINS IN PLACE CONTINUOUSLY FOR 24 HOURS OR MORE.
 - B. ADVANCE NOTICES
 - FOR THE INITIAL START OF WORK THAT REQUIRES "IMPACTS TO NORMAL TRAFFIC FLOW", THE CONTRACTOR IS TO NOTIFY THE RE IN WRITING, ON THE ADVANCE FORM TO-103 PROVIDED BY THE DEPARTMENT, OF THE PROPOSED DATE. THE NOTICE IS TO BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, BEFORE THE PROPOSED DATE. START OF WORK THAT IMPACTS NORMAL TRAFFIC FLOW WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR IS TO CONFIRM, IN WRITING TO THE RE, THE PROPOSED DATE SEVEN (AND/OR FOURTEEN) CALENDAR DAYS BEFORE STARTING THE ESTABLISHMENT OF THE TRAFFIC CONTROL MEASURES FOR THE TRAFFIC IMPACT. THE CONTRACTOR IS TO IMMEDIATELY NOTIFY THE RE IF THE PROPOSED ESTABLISHMENT CANNOT BE COMPLETED ON THE PROPOSED DATE.
 - FOR A "PERMANENT LANE CLOSURE", THE CONTRACTOR IS TO NOTIFY THE RE IN WRITING, ON ADVANCE FORM TO-103, OF THE PROPOSED DATE A NEW TRAFFIC PATTERN WILL BE ESTABLISHED. THE NOTICE IS TO BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, IN ADVANCE OF THE PROPOSED DATE. START OF A NEW TRAFFIC PATTERN WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR IS TO CONFIRM, IN WRITING TO THE RE, THE PROPOSED DATE OF THE NEW TRAFFIC PATTERN SEVEN (AND/OR FOURTEEN) DAYS BEFORE STARTING TRAFFIC CONTROL MEASURES FOR THE ESTABLISHMENT OF THE NEW PATTERN. THE CONTRACTOR IS TO IMMEDIATELY NOTIFY THE RE IF THE PROPOSED ESTABLISHMENT CANNOT BE COMPLETED ON THE PROPOSED DATE.
 - STARTING THE ESTABLISHMENT OF A NEW PERMANENT TRAFFIC PATTERN IS TO BEGIN NO EARLIER THAN 11:00 PM FRIDAY AND BE COMPLETED AND READY FOR OPERATIONS BY 6:00 PM THE FOLLOWING SUNDAY. THE ESTABLISHMENT IS TO BE COMPLETED IN ACCORDANCE WITH THE LANE CLOSURE HOURS SPECIFIED IN THE CONTRACT.
 - ADVANCE NOTICES SENT PRIOR TO THE PRE-CONSTRUCTION MEETING ARE TO BE ADDRESSED TO THE CONTACT PERSON AS SPECIFIED IN SUBSECTION 101.04 OF THE SPECIAL PROVISIONS.
 - C. PROGRESS NOTICES
 - ALL "IMPACTS TO NORMAL TRAFFIC FLOW" SCHEDULED FOR THE SEVEN DAY PERIOD STARTING ON THE FOLLOWING MONDAY ARE TO BE SUBMITTED TO THE RE BY 9:00 AM OF EACH FRIDAY ON WEEKLY FORM TO-100 PROVIDED BY THE DEPARTMENT.
 - EACH DAY OF "TEMPORARY LANE CLOSURES" ARE TO BE SUBMITTED TO THE RE BY 9:00 AM THE DAY IN ADVANCE OF THE START OF THOSE OPERATIONS ON DAILY FORM TO-101 PROVIDED BY THE DEPARTMENT.
 - "TEMPORARY LANE CLOSURES" FOR WEEKENDS ARE TO BE SUBMITTED TO THE RE BY 9:00 AM ON THE IMMEDIATELY PRECEDING FRIDAY ON THE DAILY FORM TO-101 PROVIDED BY THE DEPARTMENT.
 - D. CHANGES TO THE SCHEDULED CLOSURES
 - REQUEST FOR A CHANGE TO THE TRAFFIC CONTROL REQUIREMENTS IN THE CONTRACT DOCUMENTS ARE TO BE SUBMITTED IN WRITING TO THE RE AS FOLLOWS:
 - CHANGES TO THE SCHEDULED HOURS FOR "TEMPORARY LANE CLOSURES" ARE TO BE SUBMITTED TO THE RE AT LEAST EIGHT CALENDAR DAYS IN ADVANCE OF WHEN THE CHANGE IS PROPOSED TO START.
 - OTHER PROPOSED CHANGES TO "TEMPORARY LANE CLOSURES" AND ALL CHANGES TO "PERMANENT LANE CLOSURES" ARE TO BE SUBMITTED TO THE RE AS SPECIFIED IN THE SPECIFICATIONS.
26. WHERE MILLING OR HMA PAVING IS PERFORMED AND THE LANE IS TO BE RE-OPENED TO TRAFFIC EACH DAY, APPLY TRAFFIC STRIPES AS DIRECTED BY THE RE.



PENNONI ASSOCIATES INC.
 515 Grove Street, Suite 1B
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 T 856.547.0505 F 856.547.9174
 NJ COA NO. GA28033300

ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR AND OWNER MUST BE NOTIFIED OF ANY DISCREPANCIES BEFORE PROCEEDING WITH WORK

HUGH J. DOUGHERTY
 PROFESSIONAL ENGINEER
 NEW JERSEY LICENSE NO. GE34634

Hugh Dougherty
 11/11/2022

NJDOT FY2021 MUNICIPAL AID PROGRAM
 WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

TRAFFIC CONTROL NOTES

WILLINGBORO TOWNSHIP
 1 REV DR MARTIN LUTHER KING JR RD
 WILLINGBORO, NJ 08046

NO.	DATE	REVISIONS	BY
1	11/11/2022	NJDOT REVIEW COMMENTS	SMR

TCD-1

NEW JERSEY DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS

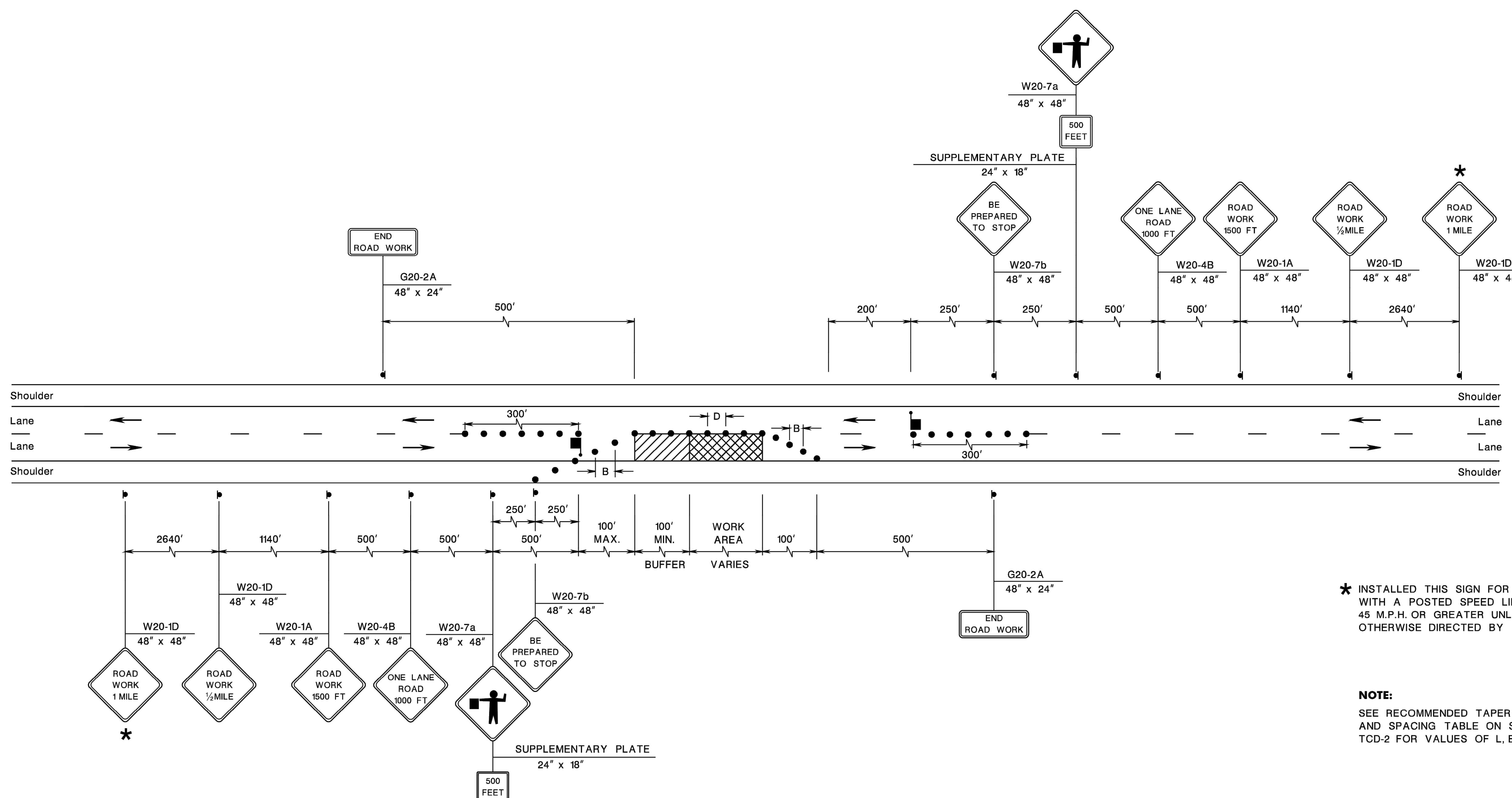
103
164

ALL DOCUMENTS PREPARED BY PENNONI ASSOCIATES ARE INSTRUMENTS OF SERVICE IN RESPECT OF THE PROJECT. THEY ARE NOT INTENDED OR REPRESENTED TO BE SUITABLE FOR REUSE BY OWNER OR OTHERS ON THE EXTENSIONS OF THE PROJECT OR ON ANY OTHER PROJECT. ANY REUSE WITHOUT WRITTEN VERIFICATION OR ADAPTATION BY PENNONI ASSOCIATES FOR THE SPECIFIC PURPOSE INTENDED WILL BE AT OWNERS SOLE RISK AND WITHOUT LIABILITY OR LEGAL EXPOSURE TO PENNONI ASSOCIATE. OWNER SHALL HOLD AND HOLD HARMLESS PENNONI ASSOCIATES FROM ALL CLAIMS, DAMAGES, LOSSES AND EXPENSES ARISING OUT OF OR RESULTING THEREFROM.

PROJECT: WTCEX21027
 DATE: DECEMBER 14, 2021
 DRAWING SCALE: NTS
 DRAWN BY: SMR
 APPROVED BY: HJD

CM6501
 SHEET 8 OF 10

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 PLOTTED: 11/11/2022 10:38 AM BY: Sami Rahay
 PROJECT STATUS: ---
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2 LANES, UNDIVIDED, LANE AND SHOULDER CLOSING WITH FLAGGING

★ INSTALLED THIS SIGN FOR ROADS WITH A POSTED SPEED LIMIT OF 45 M.P.H. OR GREATER UNLESS OTHERWISE DIRECTED BY THE RE.

NOTE:
SEE RECOMMENDED TAPER LENGTH AND SPACING TABLE ON SHEET TCD-2 FOR VALUES OF L, B, AND D.

N.T.S.

TCD-4

NEW JERSEY DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS

106
164

ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR AND OWNER MUST BE NOTIFIED OF ANY DISCREPANCIES BEFORE PROCEEDING WITH WORK

HUGH J. DOUGHERTY
 PROFESSIONAL ENGINEER
 NEW JERSEY LICENSE NO. GE34634

NJDOT FY2021 MUNICIPAL AID PROGRAM
 WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

TRAFFIC CONTROL DETAILS

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 1 REV DR MARTIN LUTHER KING JR RD
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