

BUCKINGHAM DRIVE CULVERT

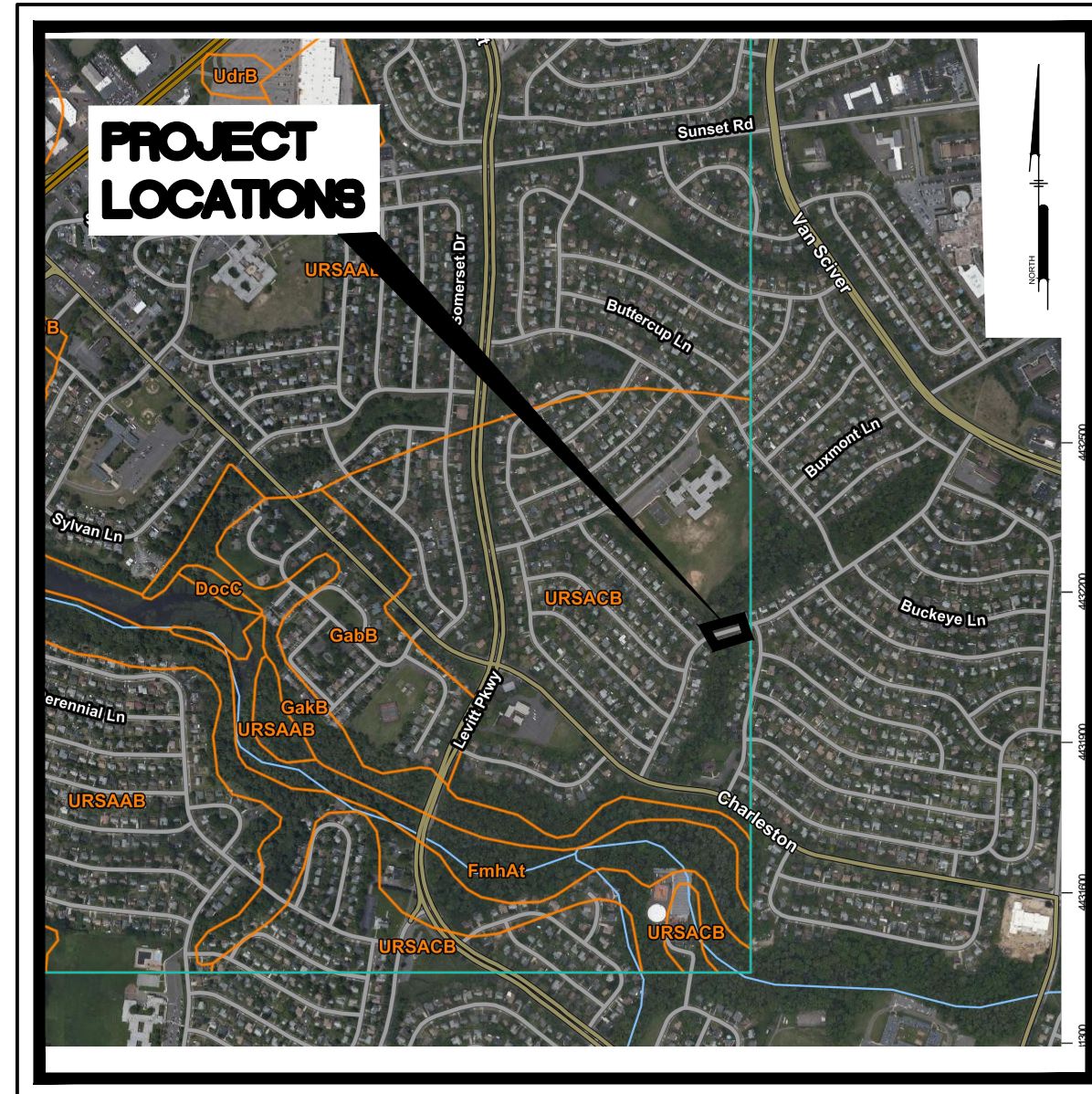
TOWNSHIP OF WILLINGBORO, BURLINGTON COUNTY, NEW JERSEY

BUCKINGHAM DRIVE

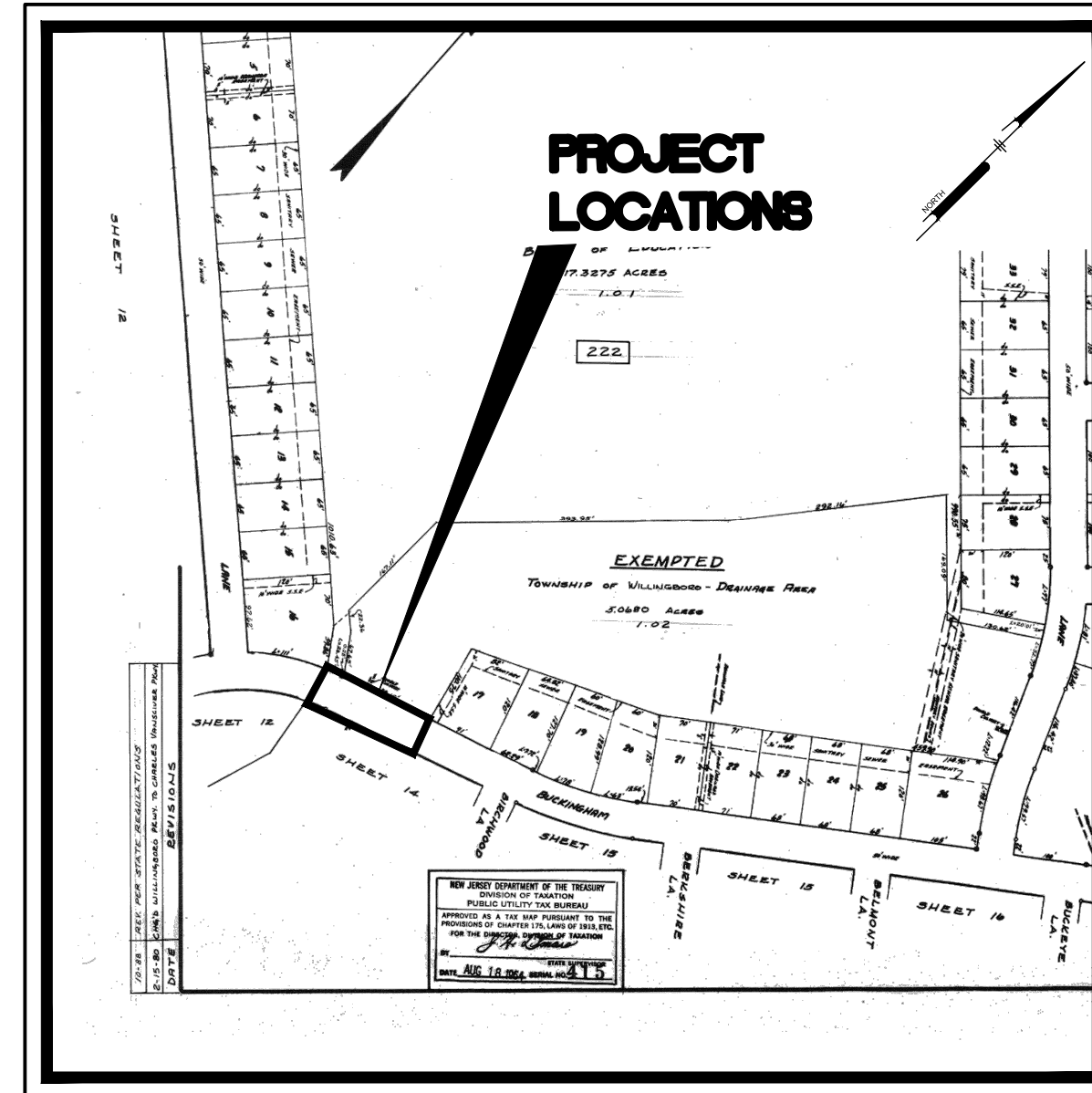
PREPARED FOR:
OWNER

TOWNSHIP OF WILLINGBORO

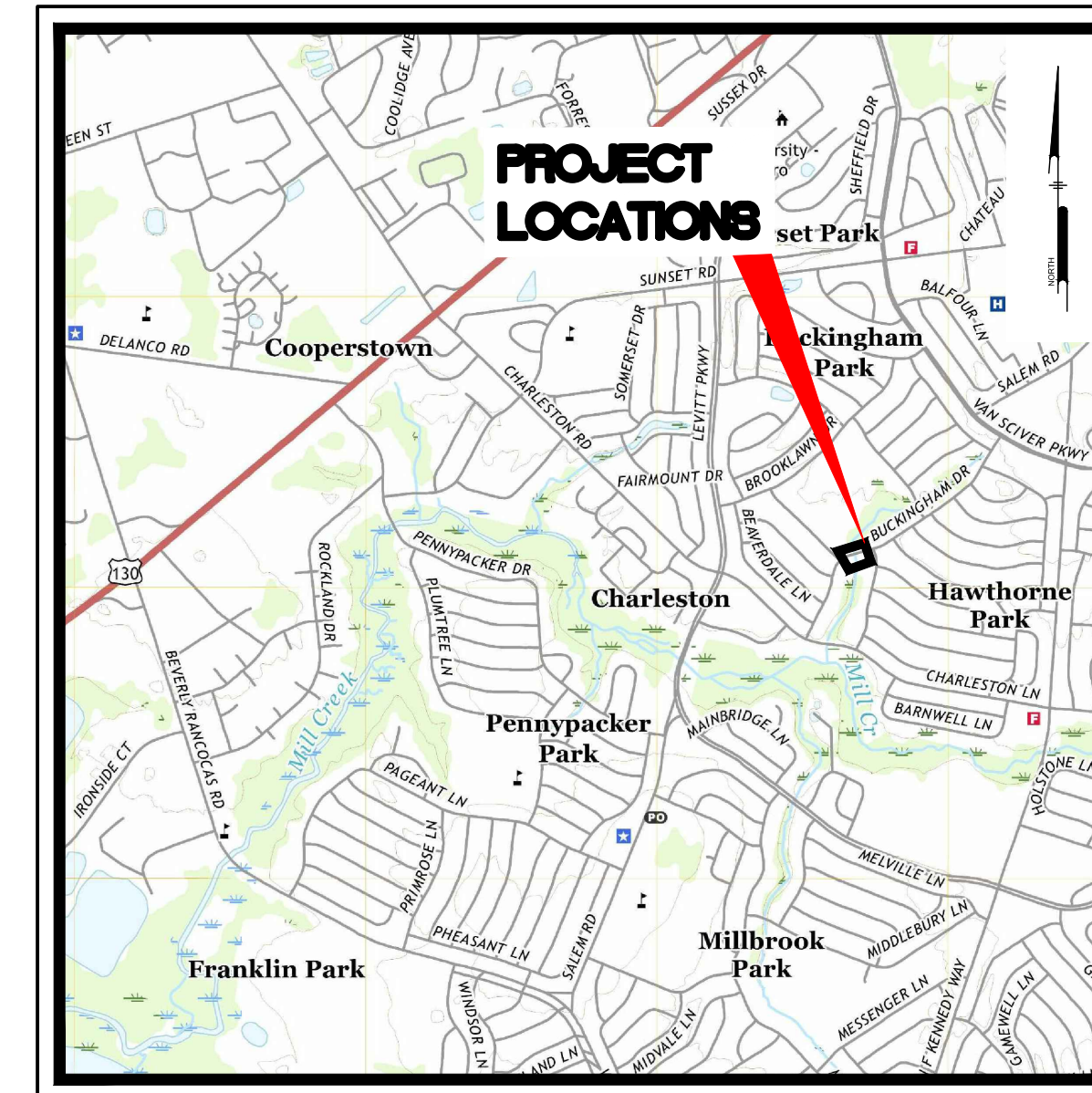
1 REV DR MARTIN LUTHER KING JR DRIVE
WILLINGBORO, NJ 08046



SOILS MAP
Scale: NTS



LOCATION MAP
Scale: NTS



USGS MAP
Scale: 1" = 2000'

PREPARED BY:

PENNONI ASSOCIATES INC.



515 Grove Street, Suite 1B
Haddon Heights, NJ 08035
T 856.547.0505
F 856.547.9174

NEW JERSEY



CALL BEFORE YOU DIG
1-800-272-1000

CONTRACTOR SHALL BE RESPONSIBLE TO COMPLY WITH THE REQUIREMENTS OF THE NEW JERSEY ONE-CALL DAMAGE PREVENTION SYSTEM AS STATED IN THE "UNDERGROUND FACILITY PROTECTION ACT".

ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR AND OWNER MUST BE NOTIFIED OF ANY DISCREPANCIES BEFORE PROCEEDING WITH WORK

HUGH J. DOUGHERTY
PROFESSIONAL ENGINEER
NEW JERSEY LICENSE NO. GE34634

BUCKINGHAM DRIVE CULVERT
WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

COVER SHEET / KEY PLAN

WILLINGBORO TOWNSHIP
1 REV DR MARTIN LUTHER KING JR DR
WILLINGBORO, NJ 08046

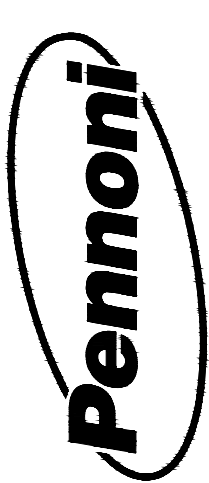
NO.	DATE	REVISIONS	BY

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PROJECT: WTCEX21028
DATE: APRIL 7, 2022
DRAWING SCALE: AS NOTED
DRAWN BY: SMR
APPROVED BY: HJD

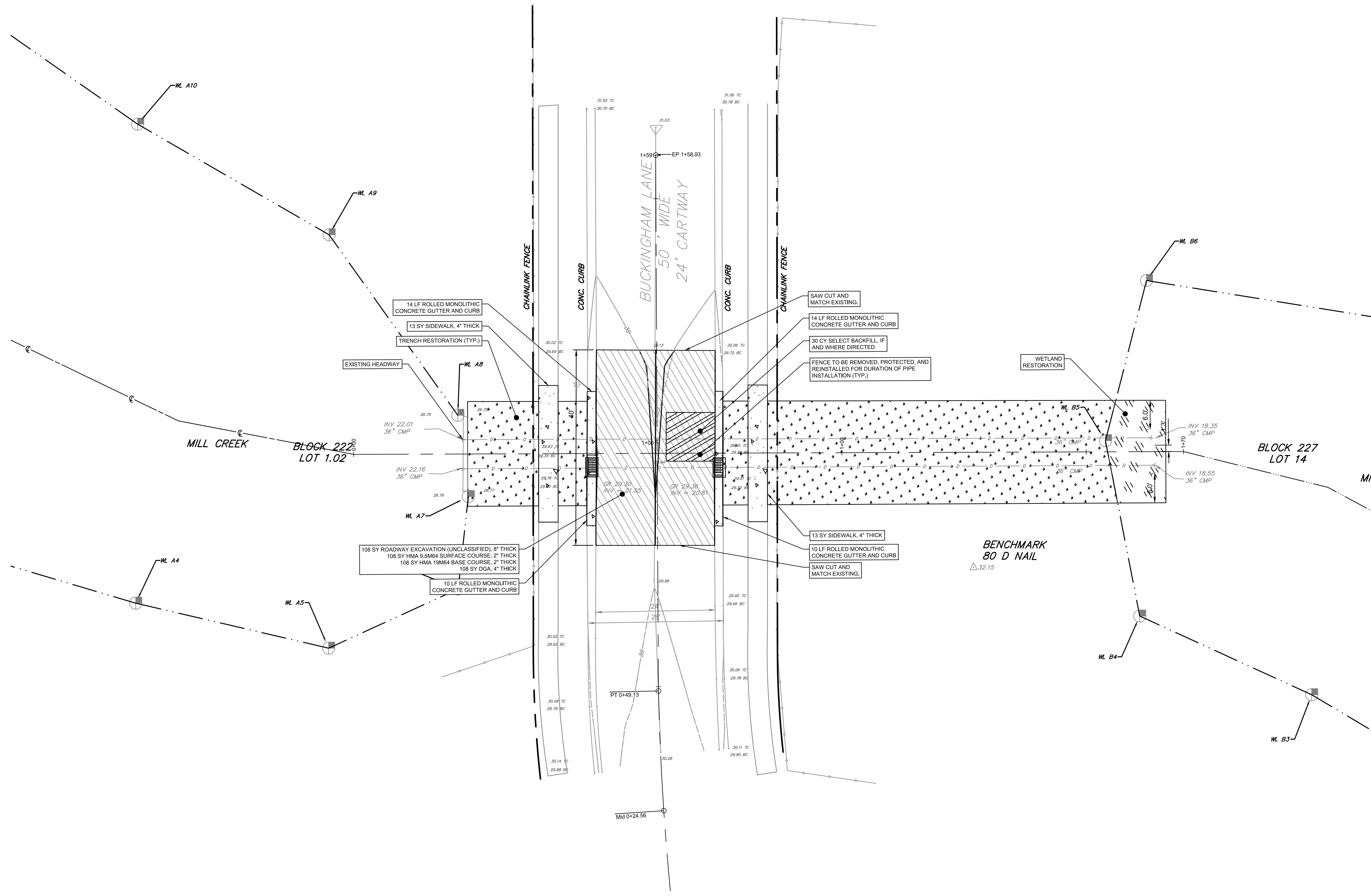
CM0001

SHEET 1 OF 8



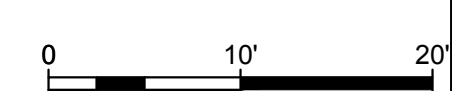
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NJ COA. NO. GA28033300

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BUCKINGHAM DRIVE
SCALE: 1" = 10'

- GENERAL NOTES:**
- FOR THE DURATION OF THE PROJECT, THE STREAM FLOW SHALL BE MAINTAINED BY THE CONTRACTORS METHODS. PAYMENT FOR THIS SHALL BE INCLUDED IN VARIOUS ITEMS OF THE BID.
 - ALL COSTS FOR FENCE REMOVAL, PROTECTION, AND REINSTALL SHALL BE INCLUDED IN THE BID PRICES FOR VARIOUS ITEMS OF THE BID.



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BUCKINGHAM DRIVE CULVERT
WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

NO.	DATE	REVISIONS	BY

PROJECT: **WTCEX21028**
DATE: **APRIL 7, 2022**
DRAWING SCALE: **1"=10'**
DRAWN BY: **SMR**
APPROVED BY: **HJD**

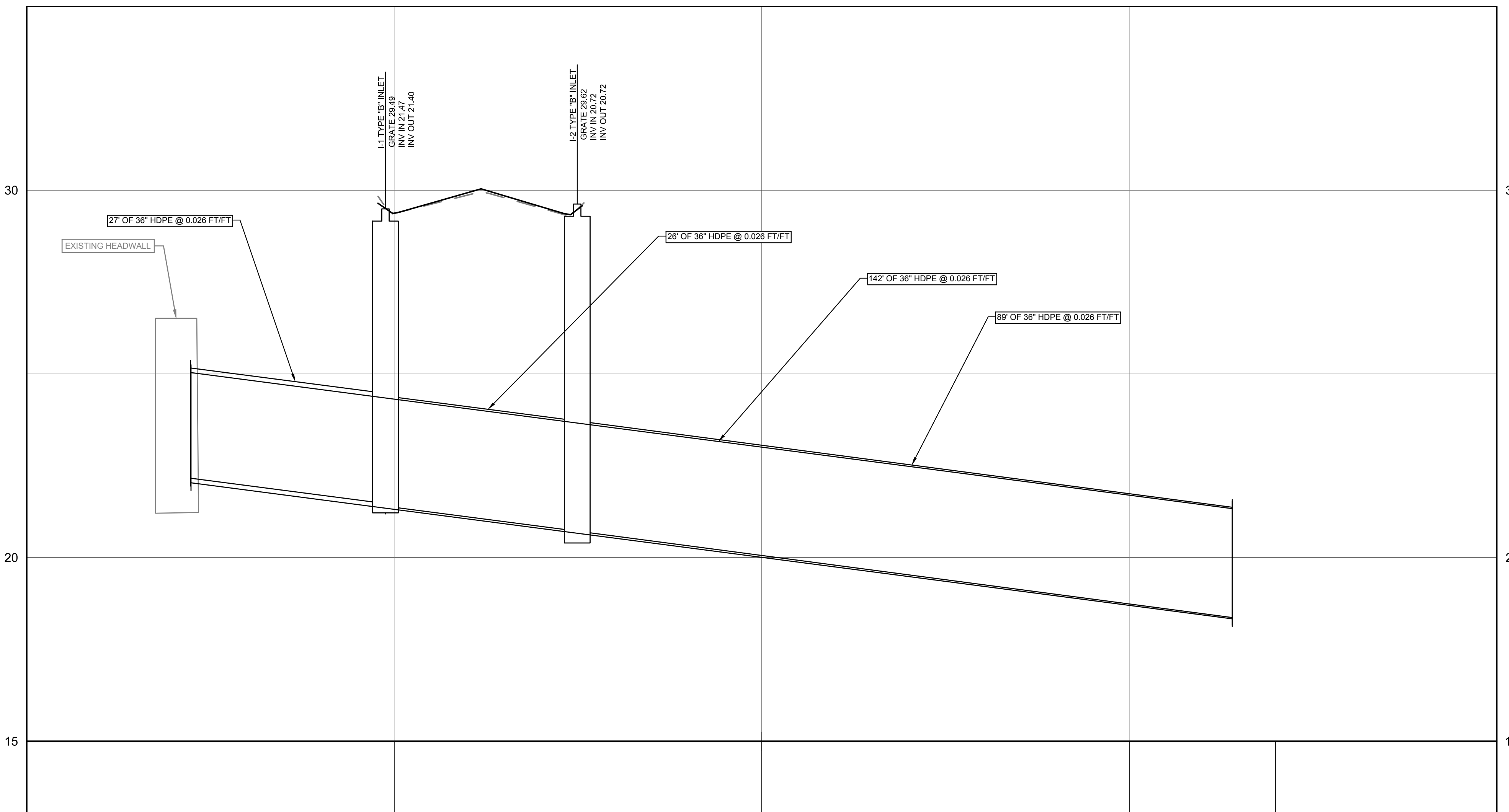
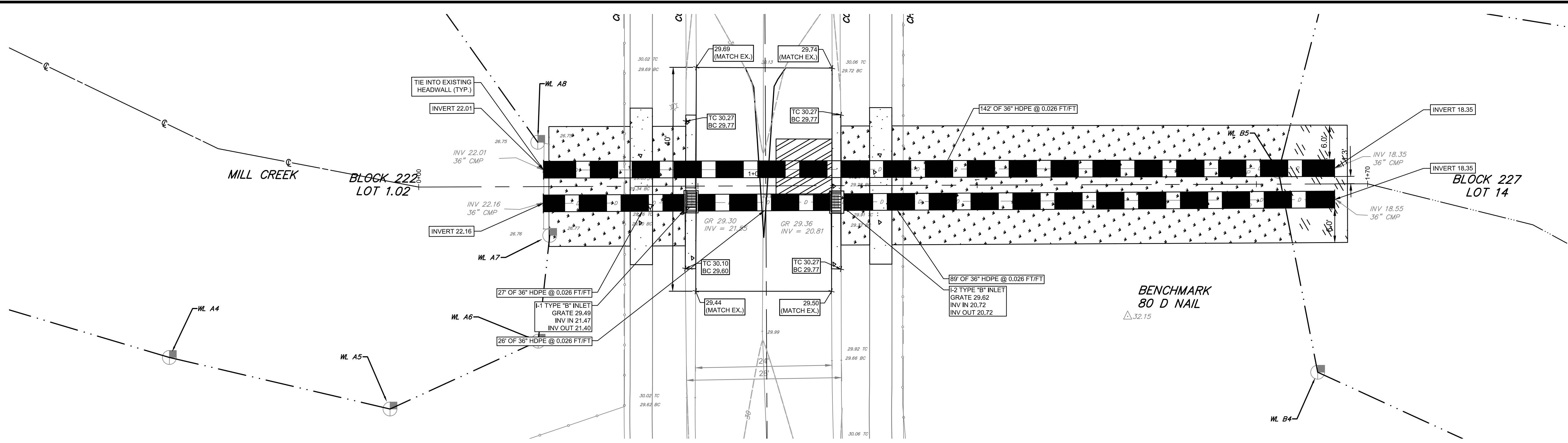
CM1001
SHEET **3** OF **8**

Pennoni
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NJ COA. NO. G428033300

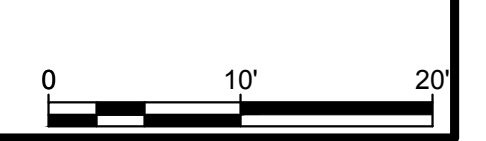
HUGH J. DOUGHERTY
PROFESSIONAL ENGINEER
NEW JERSEY LICENSE NO. GE34634
Hugh Dougherty

BUCKINGHAM DRIVE SITE PLAN
WILLINGBORO TOWNSHIP
1 REV DR MARTIN LUTHER KING JR RD
WILLINGBORO, NJ 08046

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 PLOTTED: 4/12/22 1:17 PM BY: Sean Harty



STORM MAIN ALIGNMENT
 HORIZONTAL SCALE: 1" = 10'
 VERTICAL SCALE: 1" = 2'



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 NJ COA NO. G428033300

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 NEW JERSEY LICENSE NO. GE34634

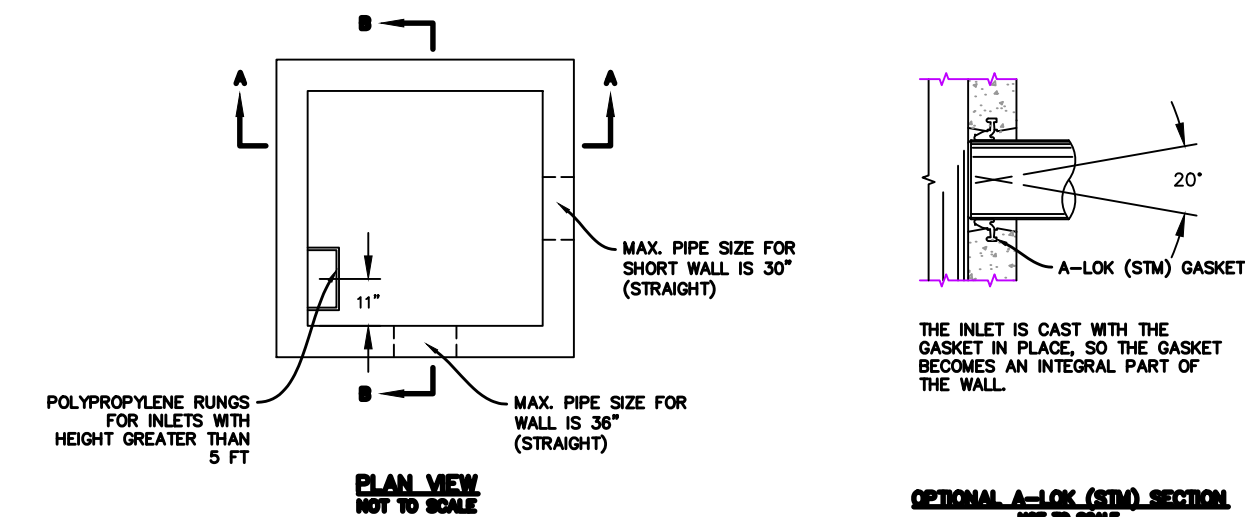
BUCKINGHAM DRIVE CULVERT
 WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY
BUCKINGHAM DRIVE - GRADING PLAN
 WILLINGBORO TOWNSHIP
 1 REV DR MARTIN LUTHER KING JR RD
 WILLINGBORO, NJ 08046

NO.	DATE	REVISIONS	BY

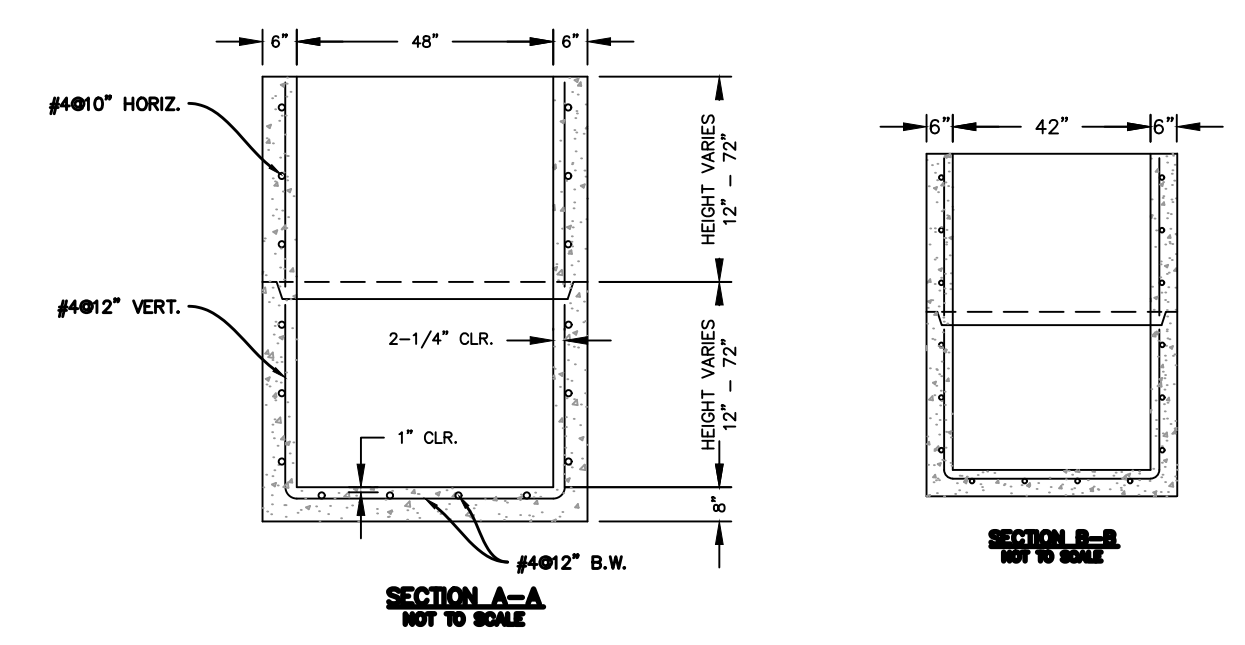
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DATE	APRIL 7, 2022
DRAWING SCALE	1"=10'
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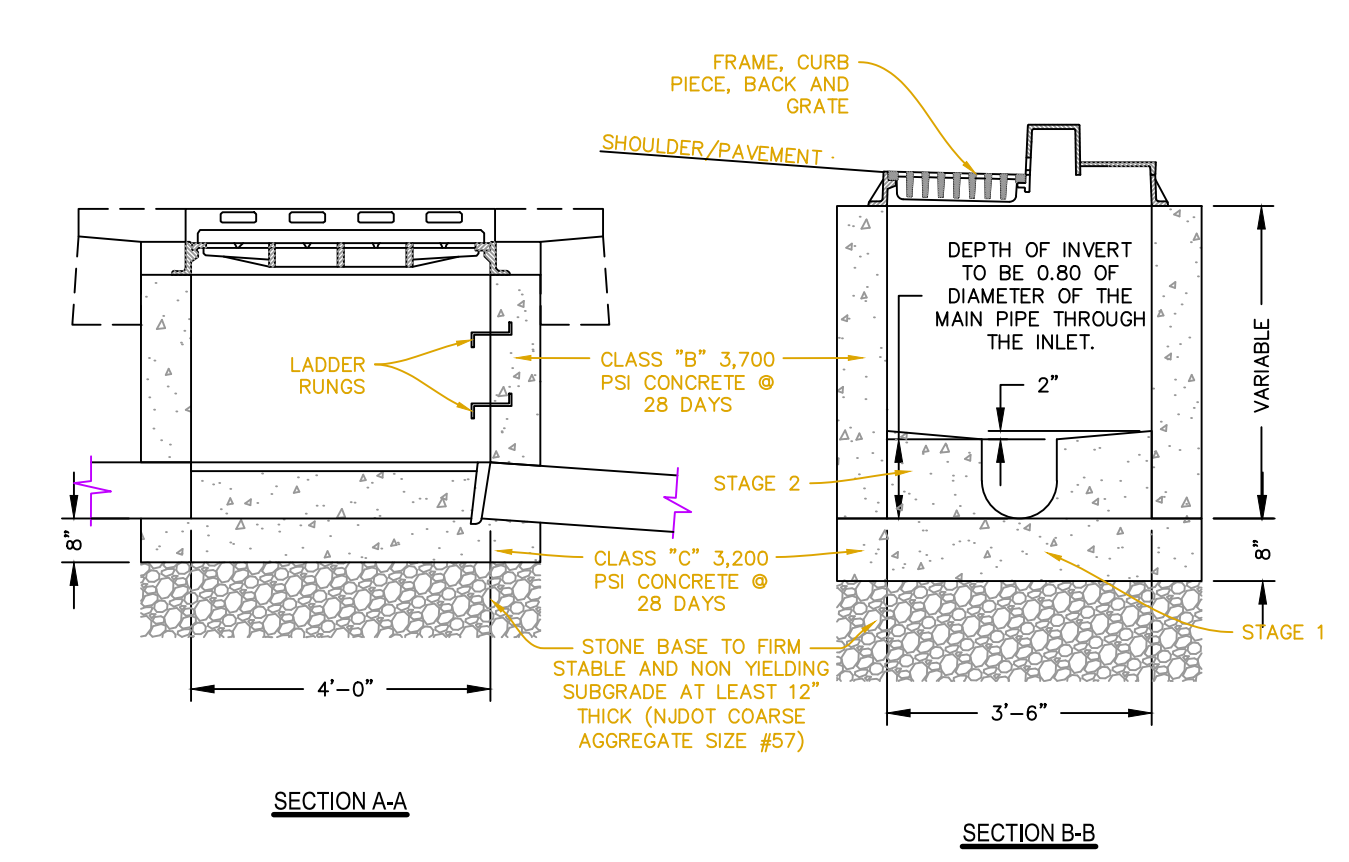
U:\Account\WTC\WTC21028 - Buckingham Lane Culvert\CD\CD1028 - Buckingham Lane Culvert\CD\CD1028.dwg PLOTFILE: Pennon\CD1028.dwg PLOTFILE: Pennon\CD1028.dwg PROJECT: ST1028



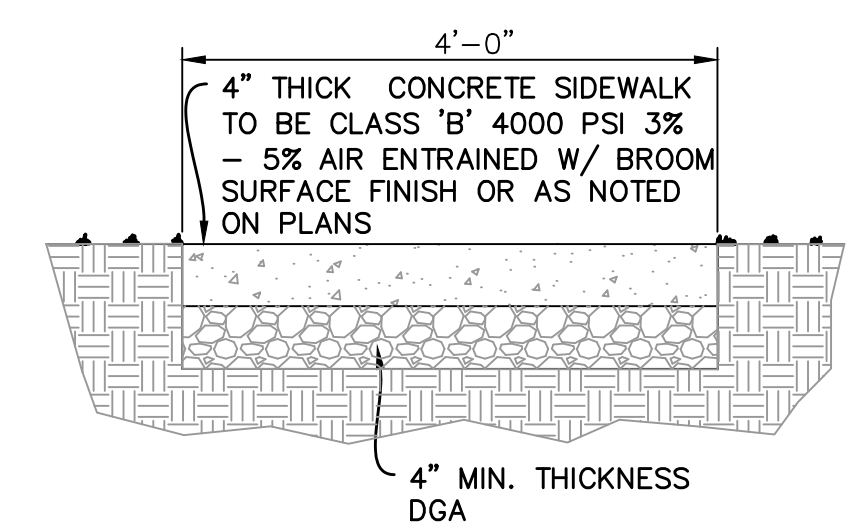
- SPECIFICATIONS:**
1. THE CONCRETE IS DESIGNED TO OBTAIN A STRENGTH OF 4,000 PSI IN 28 DAYS.
 2. THE REINFORCING STEEL HAS A YIELD STRENGTH OF 60,000 PSI.
 3. THE INLET SHALL BE DESIGNED TO MEET ASTM C-913, "PRECAST CONCRETE WATER AND WASTEWATER STRUCTURES."
 4. THE REINFORCED CONCRETE WALLS AND BASE SLAB ARE DESIGNED FOR A MAXIMUM DEPTH OF 10 FT (DEPTH = F.S. TO TOP OF BASE SLAB).
 5. STRUCTURE IS DESIGNED TO MEET NJDOT SPECS.
 6. THE OPTIONAL A-LOK STORM GASKET IS DESIGNED TO MEET THE REQUIREMENTS OF ASTM C-1478, "STORM DRAIN RESILIENT CONNECTORS BETWEEN REINFORCED CONCRETE STORM SEWER STRUCTURES, PIPES, AND LATERALS."
 7. CONSTRUCT CHANNEL AS REQUIRED.



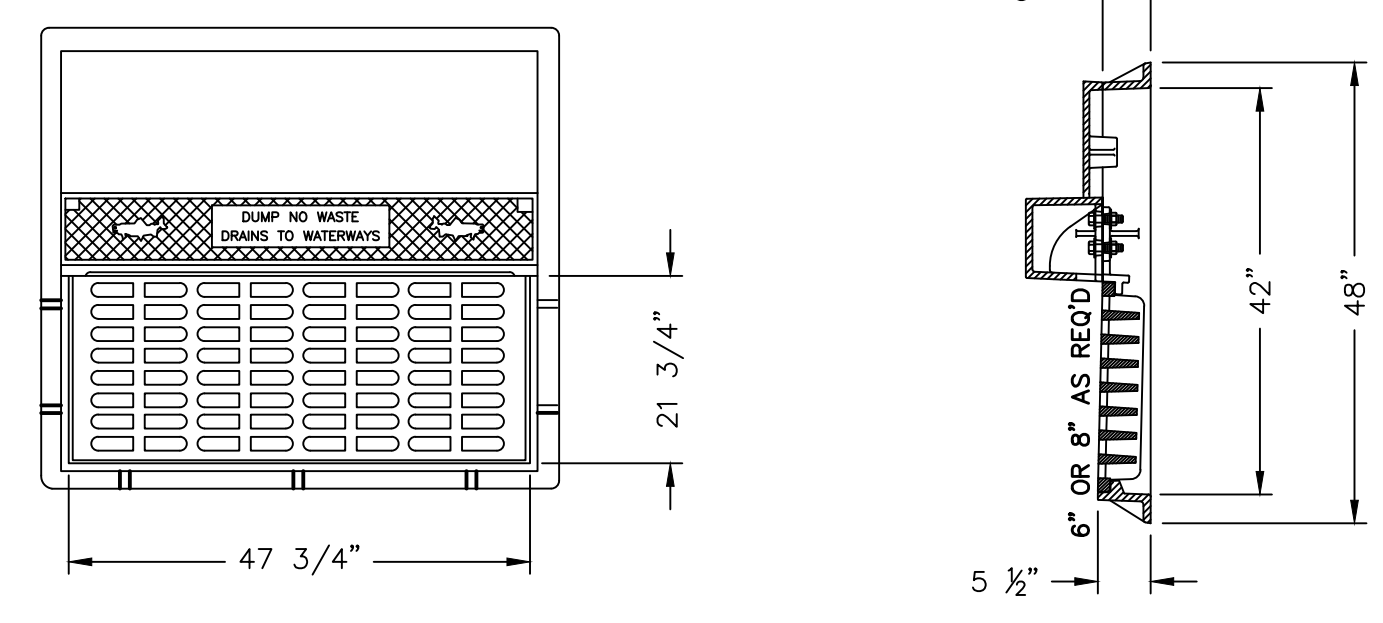
NJDOT TYPE "B" & "E" - SHALLOW INLET BASE AND RISER
N.T.S.



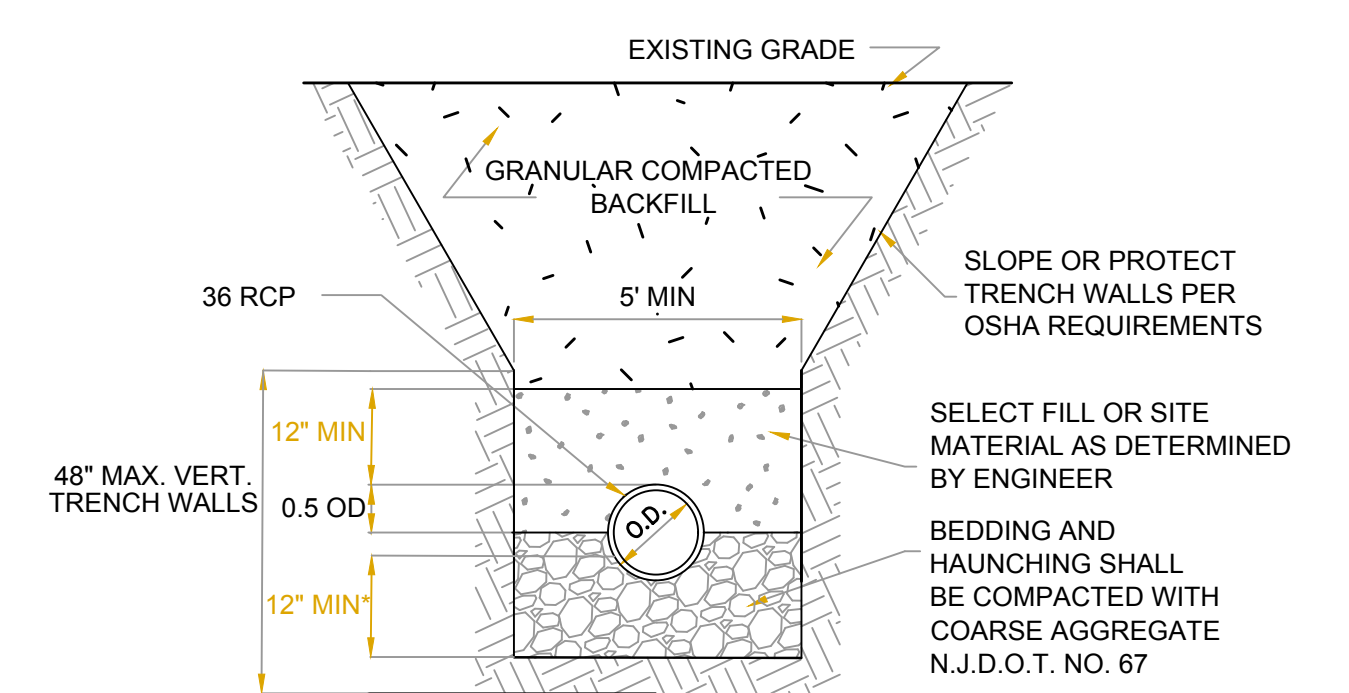
TYPE "B" NJDOT INLET DETAIL
N.T.S.



CONCRETE SIDEWALK DETAIL
N.T.S.

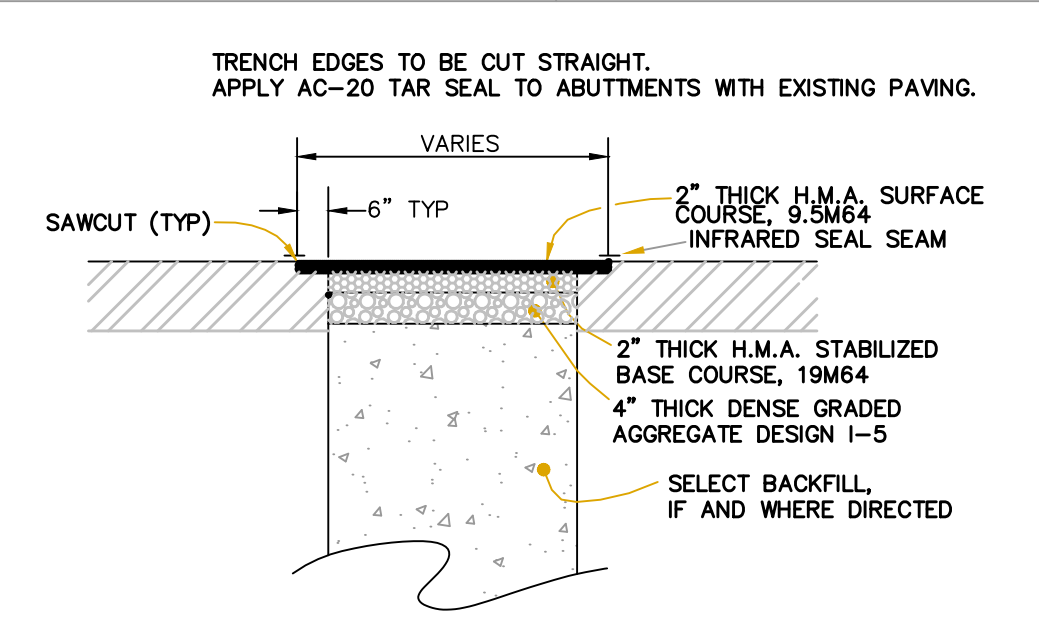


TYPE "B" CURB INLET WITH BICYCLE SAFE GRATE
N.T.S.

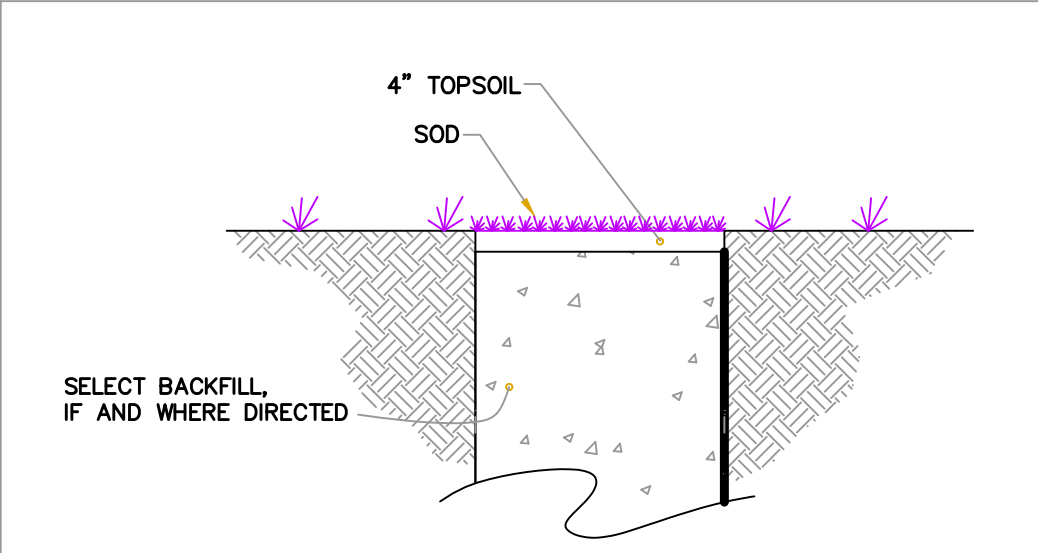


- NOTES:**
1. TRENCHING OPERATIONS SHALL CONFORM TO ALL OSHA REQUIREMENTS
 2. *BEDDING SHALL BE 12" DEPTH FOR COMBINED MAINS AND 6" DEPTH FOR ALL OTHERS INCLUDING LATERALS

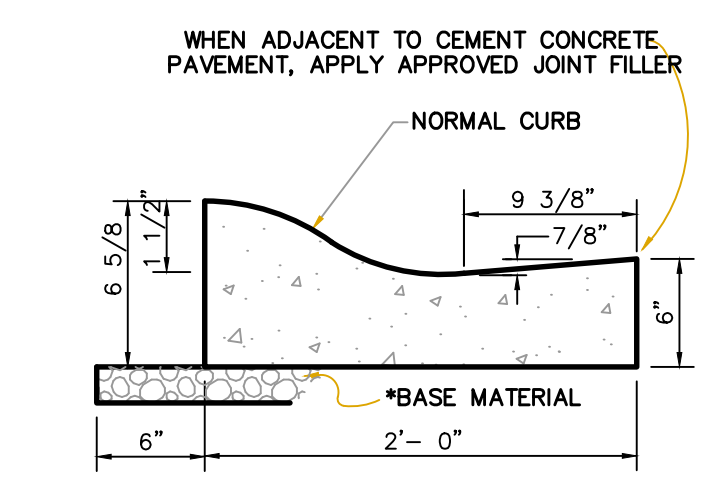
STORM DRAINAGE MAIN TRENCH DETAIL
N.T.S.



PAVED TRENCH RESTORATION
N.T.S.



GRASSED TRENCH RESTORATION
N.T.S.



ROLLED CURB DETAIL
N.T.S.

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HUGH J. DOUGHERTY
PROFESSIONAL ENGINEER
NEW JERSEY LICENSE NO. GE34634

Hugh Dougherty

BUCKINGHAM DRIVE CULVERT
WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

CONSTRUCTION DETAILS


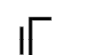




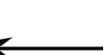










WILLINGBORO TOWNSHIP
1 REV DR MARTIN LUTHER KING JR RD
WILLINGBORO, NJ 08046

NO.	DATE	REVISIONS	BY

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PROJECT: **WTCEX21028**
DATE: **APRIL 7, 2022**
DRAWING SCALE: **N.T.S.**
DRAWN BY: **SMR**
APPROVED BY: **HJD**

LEGEND

-  BREAKAWAY BARRICADES
-  BREAKAWAY BARRICADES WITH SIGN
-  CONSTRUCTION SIGNS
-  DRUMS
-  CONE
-  CONSTRUCTION BARRIER CURB (TYPE SPECIFIED)
-  DIRECTION OF TRAFFIC FLOW
-  TRAFFIC DIRECTOR, FLAGGER
-  TRAILER MOUNTED MOUNTED ARROW BOARD SHOWING CAUTION MODE
-  ILLUMINATED FLASHING ARROW MOUNTED ON TOWING VEHICLE SHOWING ARROW PATTERN (LEFT, RIGHT, BOTH)
-  TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING CAUTION MODE
-  TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING ARROW PATTERN (LEFT, RIGHT, BOTH)
-  TEMPORARY CRASH CUSHION, INERTIAL BARRIER SYSTEM
-  TEMPORARY CRASH CUSHION, (ALL OTHER APPROVED)
-  BUFFER ZONE
-  WORK AREA
-  PAINT STRIPING TRUCK OR OTHER OPERATING VEHICLE

GENERAL NOTES:

1. ADVANCE WARNING SIGNS DISTANCES AND TAPER LENGTHS MAY BE EXTENDED, AT DIRECTION OF THE DEPARTMENT, TO ADJUST FOR REDUCED VISIBILITY DUE TO HORIZONTAL AND VERTICAL CURVATURE OF THE ROADWAY.
2. THE APPROXIMATE LOCATIONS OF THE ILLUMINATED FLASHING ARROW BOARDS ARE SHOWN ON THE TRAFFIC CONTROL PLANS. THESE LOCATIONS MAY BE MODIFIED AS APPROVED BY RE TO ADJUST FOR VISIBILITY DUE TO HORIZONTAL OR VERTICAL CURVATURE OF THE ROADWAY OR TO POSITION AT A SAFER LOCATION. ILLUMINATED FLASHING ARROW BOARDS ARE TO BE USED FOR TEMPORARY LANE CLOSINGS AND AT LOCATIONS SHOWN ON THE TRAFFIC CONTROL PLANS.
3. PRIOR TO ANY ROAD CONSTRUCTION, TRAFFIC CONTROL SIGNS AND DEVICES ARE TO BE IN PLACE.
4. RAMPS AND/OR SIDE STREETS ENTERING THE ROADWAY AFTER THE FIRST ADVANCE WARNING SIGN ARE TO BE PROVIDED WITH AT LEAST ONE W20-F SIGN (ROAD WORK AHEAD) AS A MINIMUM.
5. ALL EXISTING ROAD SIGNS, PAVEMENT MARKINGS, AND /OR PLOWABLE PAVEMENT REFLECTORS WHICH CONFLICT WITH THE PROPOSED TRAFFIC CONTROL PLAN ARE TO BE COVERED, REMOVED, OR RELOCATED AS DIRECTED BY THE RE.
6. CONFLICTING OR NON-OPERATING SIGNAL INDICATIONS ON EITHER THE EXISTING, TEMPORARY, OR PROPOSED TRAFFIC SIGNAL SYSTEMS ARE TO BE BAGGED OR COVERED.
7. MAINTENANCE AND PROTECTION OF TRAFFIC TO BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES - PART VI "STANDARDS AND GUIDES FOR TRAFFIC CONTROL FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, UTILITY, AND INCIDENT MANAGEMENT OPERATIONS", UNLESS OTHERWISE NOTED IN THE PLANS AND SPECIFICATIONS.
8. CONSTRUCTION SIGN W99-2 (GIVE US A BRAKE) TO BE LOCATED 200 FEET IN ADVANCE OF PROJECT LIMITS.
9. A W1-6 (ARROW) SIGN MOUNTED ON A BREAKAWAY BARRICADE AND CENTERED ON THE CLOSED WIDTH TO BE LOCATED 100 FEET BEYOND EACH INTERSECTION OR MAIN ACCESS POINT WITHIN THE AREA OF A LANE OR SHOULDER CLOSURE.
10. CONSTRUCTION SIGNS R11-4 (ROAD CLOSED THRU TRAFFIC) TO BE PLACED AT THE INTERSECTING STREETS WHICH ARE CLOSED TO TRAFFIC BECAUSE OF CONSTRUCTION.
11. CONSTRUCTION SIGNS W8-9A (SYMBOL FOR UNEVEN PAVEMENT) AND W8-14A (GROOVED PAVEMENT) TO BE USED WHEN SUCH PAVEMENT CONDITIONS EXIST.
12. MOVING WORK AREAS IN A LANE CLOSURE REQUIRE A TRAILER MOUNTED ILLUMINATED FLASHING ARROW TO REMAIN AT THE END OF THE TAPER, THE TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION THAT IS TO MOVE WITH THE WORK AREAS TO KEEP A 70 FEET MIN. AND 150 FEET MAX. BUFFER IN ADVANCE OF EACH WORK AREA.
13. THE CONTRACTOR TO SUBMIT A PLAN FOR THE SAFE ACCESS OF CONSTRUCTION VEHICLES THROUGHOUT THE WORK SITE WHERE SPACE CONSTRAINTS PREVENT THE USE OF LANE CLOSURES. THE PLAN TO BE SUBMITTED TO THE RE AS SPECIFIED IN THE SPECIFICATIONS.
14. BACKFILL ALL EXCAVATED AREAS WITHIN OR ADJACENT TO THE ROADWAY AND PLACE ON AT LEAST 6H:1V SLOPE BEFORE THE END OF EACH WORK DAY. OTHER EXCAVATED AREA WITHIN THE CLEAR ZONE ARE TO BE BACKFILLED.
15. WHERE REQUIRED, THE CONTRACTOR IS TO MAKE PROVISIONS FOR MAINTAINING PEDESTRIAN CROSSING LOCATIONS AND TYPE AS DIRECTED BY THE RE.
16. BITUMINOUS CONCRETE PLACED DURING THE VARIOUS CONSTRUCTION STAGES TO BE TRANSITIONED ON A MINIMUM 20H:1V SLOPE TO MEET THE ADJACENT EXISTING GRADE AT THE LONGITUDINAL AND TRANSVERSE LIMITS OF THE STAGE CONSTRUCTION AREAS UNLESS OTHERWISE NOTED ON THE STAGE CONSTRUCTION PLANS.
17. THE PLACEMENT AND /OR RELOCATION OF CONSTRUCTION BARRIER CURB TO BE DONE DURING ALLOWABLE LANE CLOSURE HOURS.
18. CONSTRUCTION ZONE SPEED LIMIT WILL BE DETERMINED BY THE BUREAU OF TRAFFIC ENGINEERING, REGIONAL TRAFFIC ENGINEER - WORK ZONE, AT THE TIME OF OR DURING CONSTRUCTION, AS REQUESTED BY THE RE.
19. THE SPEED LIMIT, R2-1 (BLACK ON WHITE) WITH ADDED WORK ZONE PLATE (BLACK ON ORANGE) SIGNS TO BE LOCATED THROUGH WORK AREAS AS DIRECTED BY THE BUREAU OF TRAFFIC ENGINEERING, REGIONAL TRAFFIC ENGINEER - WORK ZONE.
20. THE REDUCED SPEED AHEAD SIGN, W3-5(S) (BLACK ON ORANGE) TO BE LOCATED IN ADVANCE OF SPEED LIMIT R2-1 SIGNS WHICH REDUCE THE NORMAL POSTED SPEED LIMIT THROUGH THE CONSTRUCTION ZONE.
21. TRAFFIC FINES DOUBLED IN WORK AREA R(NJ)5-17(S), 4 FEET BY 2.5 FEET SIGN TO BE LOCATED 500 FEET AFTER THE FIRST ADVANCE WARNING SIGN, (W20 SERIES) AT EACH WORK AREA LOCATED WITHIN URBAN AREAS. THIS SIGN TO ALSO BE USED ON PROJECTS REQUIRING MOVING OPERATIONS IN WHICH CASE THE SIGN IS TO BE MOUNTED ON A SLOW MOVING CONSTRUCTION VEHICLE.
22. DO NOT CONSTRUCT THE FINAL HMA SURFACE PAVEMENT UNTIL THE FINAL STAGE OF THE PROJECT UNLESS OTHERWISE DIRECTED BY THE RE OR INDICATED ON THE PLANS. SET MANHOLES AND INLETS TO FINISHED GRADE AND CONSTRUCT TEMPORARY PAVEMENT RAMPS AROUND THEM WITH A MINIMUM 20H:1V SLOPE IN ALL DIRECTIONS USING HOT MIX ASPHALT PAVEMENT. THIS TEMPORARY MATERIAL WILL BE REMOVED IMMEDIATELY PRIOR TO PLACING THE SURFACE COURSE.
23. PLACE TRAFFIC CONTROL DEVICES FOR LANE CLOSURES INCLUDING SIGNS, CONES, BARRICADES, ETC. AS SHOWN ON PLANS. NO SIGNS ARE TO BE PLACED WITHOUT ACTUAL LANE CLOSURES AND REMOVE IMMEDIATELY UPON REMOVAL OF THE CLOSURES.
24. CONES MAY BE SUBSTITUTED FOR DRUMS AND INSTALLED UPON THE APPROVAL OF THE RE.
25. TRAFFIC IMPACT NOTICES AND CHANGES
 - A. TERMS:
WHEN THE FOLLOWING TERMS ARE USED, THE INTENT AND MEANING IS AS FOLLOWS:
 - i. IMPACTS TO NORMAL TRAFFIC FLOW - WORK THAT REQUIRES A PORTION OF THE PAVED ROADWAY BEING BLOCKED OR CLOSED WITH SAFETY DEVICES OR VEHICLES, INCLUDING, BUT NOT LIMITED TO, FULL OR PARTIAL LANE CLOSURES, FULL OR PARTIAL RAMP CLOSURES, SHOULDER CLOSURES, MOVING OPERATIONS SUCH AS TRAFFIC STRIPING OR SWEEPING, LANE SHIFTS, OR ALTERNATING TRAFFIC. THIS APPLIES EVEN WHEN DETOURS ARE PROVIDED.
 - ii. TEMPORARY LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH IS ROUTINELY SET UP AND REMOVED ON A DAILY BASIS.
 - iii. PERMANENT LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH REMAINS IN PLACE CONTINUOUSLY FOR 24 HOURS OR MORE.
 - B. ADVANCE NOTICES
FOR THE INITIAL START OF WORK THAT REQUIRES "IMPACTS TO NORMAL TRAFFIC FLOW", THE CONTRACTOR IS TO NOTIFY THE RE IN WRITING, ON THE ADVANCE FORM TO-103 PROVIDED BY THE DEPARTMENT, OF THE PROPOSED DATE. THE NOTICE IS TO BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, BEFORE THE PROPOSED DATE. START OF WORK THAT IMPACTS NORMAL TRAFFIC FLOW WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR IS TO CONFIRM, IN WRITING TO THE RE, THE PROPOSED DATE SEVEN (AND/OR FOURTEEN) CALENDAR DAYS BEFORE STARTING THE ESTABLISHMENT OF THE TRAFFIC CONTROL MEASURES FOR THE TRAFFIC IMPACT. THE CONTRACTOR IS TO IMMEDIATELY NOTIFY THE RE IF THE PROPOSED ESTABLISHMENT CANNOT BE COMPLETED ON THE PROPOSED DATE.

FOR A "PERMANENT LANE CLOSURE", THE CONTRACTOR IS TO NOTIFY THE RE IN WRITING, ON ADVANCE FORM TO-103, OF THE PROPOSED DATE A NEW TRAFFIC PATTERN WILL BE ESTABLISHED. THE NOTICE IS TO BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, IN ADVANCE OF THE PROPOSED DATE. START OF A NEW TRAFFIC PATTERN WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR IS TO CONFIRM, IN WRITING TO THE RE, THE PROPOSED DATE OF THE NEW TRAFFIC PATTERN SEVEN (AND/OR FOURTEEN) DAYS BEFORE STARTING TRAFFIC CONTROL MEASURES FOR THE ESTABLISHMENT OF THE NEW PATTERN. THE CONTRACTOR IS TO IMMEDIATELY NOTIFY THE RE IF THE PROPOSED ESTABLISHMENT CANNOT BE COMPLETED ON THE PROPOSED DATE.

STARTING THE ESTABLISHMENT OF A NEW PERMANENT TRAFFIC PATTERN IS TO BEGIN NO EARLIER THAN 11:00 PM FRIDAY AND BE COMPLETED AND READY FOR OPERATIONS BY 6:00 PM THE FOLLOWING SUNDAY. THE ESTABLISHMENT IS TO BE COMPLETED IN ACCORDANCE WITH THE LANE CLOSURE HOURS SPECIFIED IN THE CONTRACT.

ADVANCE NOTICES SENT PRIOR TO THE PRE-CONSTRUCTION MEETING ARE TO BE ADDRESSED TO THE CONTACT PERSON AS SPECIFIED IN SUBSECTION 101.04 OF THE SPECIAL PROVISIONS.
 - C. PROGRESS NOTICES

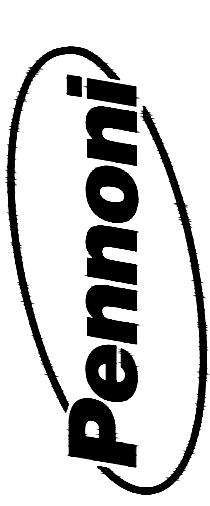
ALL "IMPACTS TO NORMAL TRAFFIC FLOW" SCHEDULED FOR THE SEVEN DAY PERIOD STARTING ON THE FOLLOWING MONDAY ARE TO BE SUBMITTED TO THE RE BY 9:00 AM OF EACH FRIDAY ON WEEKLY FORM TO-100 PROVIDED BY THE DEPARTMENT.

EACH DAY OF "TEMPORARY LANE CLOSURES" ARE TO BE SUBMITTED TO THE RE BY 9:00 AM THE DAY IN ADVANCE OF THE START OF THOSE OPERATIONS ON DAILY FORM TO-101 PROVIDED BY THE DEPARTMENT.

"TEMPORARY LANE CLOSURES" FOR WEEKENDS ARE TO BE SUBMITTED TO THE RE BY 9:00 AM ON THE IMMEDIATELY PRECEDING FRIDAY ON THE DAILY FORM TO-101 PROVIDED BY THE DEPARTMENT.
 - D. CHANGES TO THE SCHEDULED CLOSURES
REQUEST FOR A CHANGE TO THE TRAFFIC CONTROL REQUIREMENTS IN THE CONTRACT DOCUMENTS ARE TO BE SUBMITTED IN WRITING TO THE RE AS FOLLOWS:

CHANGES TO THE SCHEDULED HOURS FOR "TEMPORARY LANE CLOSURES" ARE TO BE SUBMITTED TO THE RE AT LEAST EIGHT CALENDAR DAYS IN ADVANCE OF WHEN THE CHANGE IS PROPOSED TO START.


OTHER PROPOSED CHANGES TO "TEMPORARY LANE CLOSURES" AND ALL CHANGES TO "PERMANENT LANE CLOSURES" ARE TO BE SUBMITTED TO THE RE AS SPECIFIED IN THE SPECIFICATIONS.
26. WHERE MILLING OR HMA PAVING IS PERFORMED AND THE LANE IS TO BE RE-OPENED TO TRAFFIC EACH DAY, APPLY TRAFFIC STRIPES AS DIRECTED BY THE RE.



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ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR AND OWNER MUST BE NOTIFIED OF ANY DISCREPANCIES BEFORE PROCEEDING WITH WORK

HUGH J. DOUGHERTY
PROFESSIONAL ENGINEER
NEW JERSEY LICENSE NO. GE34634



BUCKINGHAM DRIVE CULVERT
WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

TRAFFIC CONTROL NOTES

WILLINGBORO TOWNSHIP
1 REV. DR. MARTIN LUTHER KING, JR. RD.
WILLINGBORO, NJ 08046

NO.	DATE	REVISIONS	BY

TCD-1

NEW JERSEY DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS

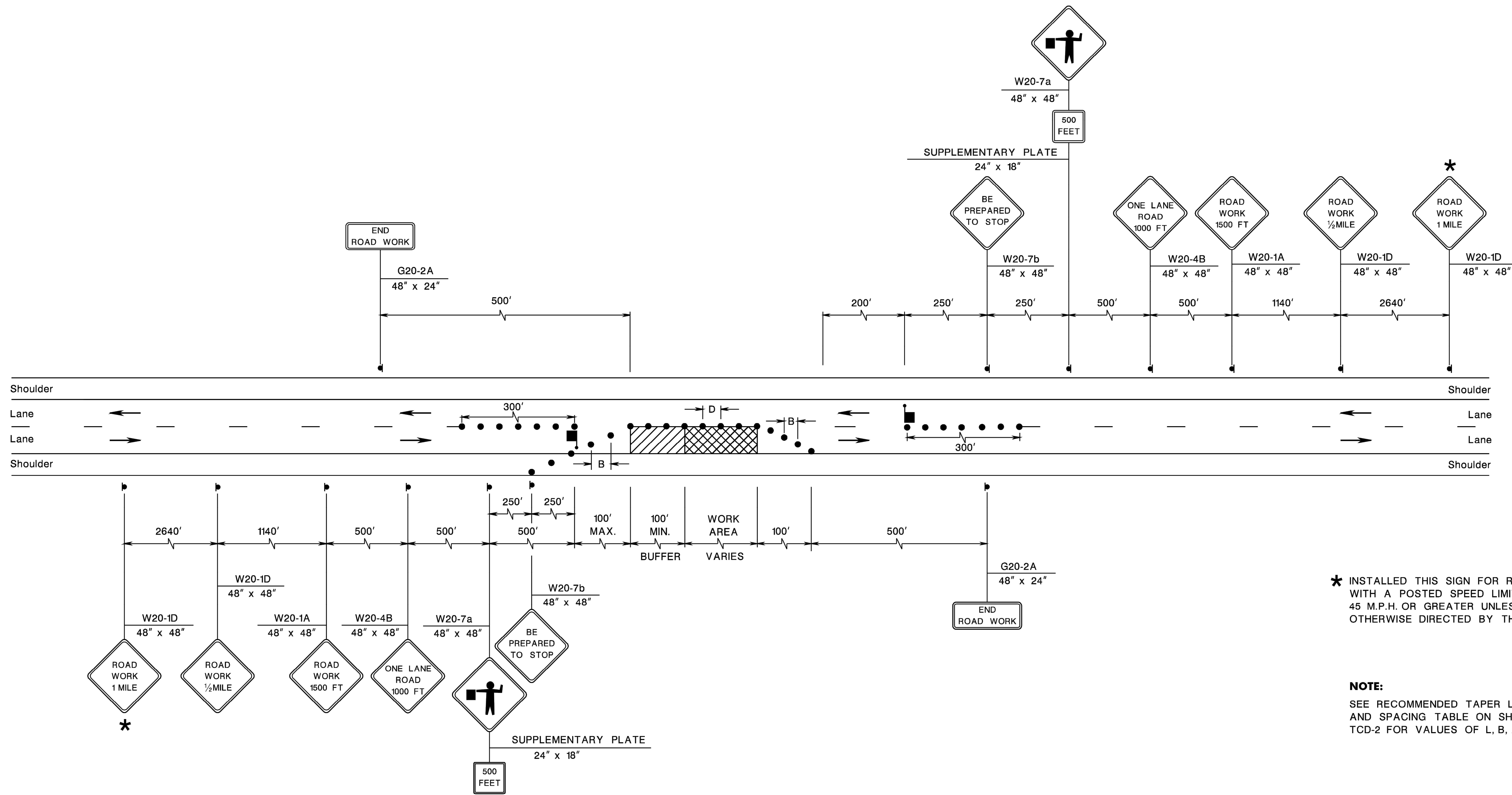
103
164

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PROJECT	WTCEX21028
DATE	APRIL 7, 2022
DRAWING SCALE	NTS
DRAWN BY	SMR
APPROVED BY	HJD

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 PROJECT: 1028



★ INSTALLED THIS SIGN FOR ROADS WITH A POSTED SPEED LIMIT OF 45 M.P.H. OR GREATER UNLESS OTHERWISE DIRECTED BY THE RE.

NOTE:
SEE RECOMMENDED TAPER LENGTH AND SPACING TABLE ON SHEET TCD-2 FOR VALUES OF L, B, AND D.

2 LANES, UNDIVIDED, LANE AND SHOULDER CLOSING WITH FLAGGING

N.T.S.

TCD-4

NEW JERSEY DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS

106
164

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TRAFFIC CONTROL DETAILS

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 1 REV DR MARTIN LUTHER KING JR RD
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PROJECT: WTCEX21028
 DATE: APRIL 7, 2022
 DRAWING SCALE: NTS
 DRAWN BY: SMR
 APPROVED BY: HUD

GENERAL

THE CONTRACTOR SHALL SCHEDULE AND CONDUCT HIS OPERATIONS TO MINIMIZE EROSION OF SOILS AND TO PREVENT SILTING AND MUDDYING OF STREAMS, RIVERS, IRRIGATION SYSTEMS AND IMPOUNDMENTS (LAKES, RESERVOIRS, ETC.). CONSTRUCTION OF DRAINAGE FACILITIES AND PERFORMANCE OF THEIR CONTRACT WORK WHICH WILL CONTRIBUTE TO THE CONTROL OF EROSION AND SEDIMENTATION SHALL BE CARRIED OUT IN CONJUNCTION WITH EARTHWORK OPERATIONS OR AS SOON THEREAFTER AS PRACTICABLE. THE AREA OF BARE SOIL EXPOSED AT ANY ONE TIME BY CONSTRUCTION OPERATIONS SHALL BE KEPT TO A MINIMUM.

PRIOR TO A SUSPENSION OF CONSTRUCTION OPERATIONS FOR A PERIOD OF 20 DAYS OR MORE, ANY AREA THAT HAS BEEN DISTURBED AND LEFT EXPOSED SHALL RECEIVE TEMPORARY STABILIZATION.

WHEN BORROW MATERIAL IS OBTAINED FROM OTHER THAN COMMERCIAL OPERATED SOURCES, EROSION OF THE BORROW SITE SHALL BE SO CONTROLLED BOTH DURING AND AFTER COMPLETION OF THE WORK THAT EROSION WILL BE MINIMIZED AND SEDIMENT WILL NOT ENTER STREAMS OR OTHER BODIES OF WATER. WASTE OR DISPOSAL AREAS AND CONSTRUCTION ROADS SHALL BE LOCATED AND CONSTRUCTED IN A MANNER THAT WILL KEEP SEDIMENT FROM ENTERING STREAMS.

FREQUENT FORDING OF LIVE STREAMS WILL NOT BE PERMITTED; THEREFORE, TEMPORARY BRIDGES OR OTHER STRUCTURES SHALL BE USED WHEREVER AN APPRECIABLE NUMBER OF STREAM CROSSINGS ARE NECESSARY. UNLESS OTHERWISE APPROVED IN WRITING BY THE ENGINEERS, MECHANIZED EQUIPMENT SHALL BE NOT OPERATED IN LIVE STREAMS.

WHEN WORK AREAS OR GRAVEL PITS ARE LOCATED IN OR ADJACENT TO LIVE STREAMS, SUCH AREAS SHALL BE SEPARATED FROM THE MAIN STREAM BY A DIKE OR OTHER BARRIER TO KEEP SEDIMENT FROM ENTERING A FLOWING STREAM. CARE SHALL BE TAKEN DURING THE CONSTRUCTION AND REMOVAL OF SUCH BARRIERS TO MINIMIZE THE MUDDYING OF A STREAM.

WATER FROM AGGREGATE WASHING OR OTHER OPERATIONS CONTAINING SEDIMENT SHALL BE TREATED BY FILTRATION, A SETTLING BASIN OR OTHER MEANS SUFFICIENT TO REDUCE THE SEDIMENT CONTENT TO NOT MORE THAN THAT OF THE STREAM INTO WHICH IT IS DISCHARGED.

POLLUTANTS SUCH AS FUELS, LUBRICANTS, BITUMENS, RAW SEWAGE AND OTHER HARMFUL MATERIALS SHALL NOT BE DISCHARGED INTO OR NEAR RIVERS, STREAMS AND IMPOUNDMENTS OR INTO NATURAL OR MANMADE CHANNELS LEADING THERETO. WASH WATER OR WASTE FROM CONCRETE MIXING OPERATIONS SHALL NOT BE ALLOWED TO ENTER LIVE STREAMS.

ALL APPLICABLE REGULATIONS OF FISH AND WILDLIFE AGENCIES AND STATUTES RELATING TO THE PREVENTION AND ABATEMENT OF POLLUTION SHALL BE COMPLIED WITH IN THE PERFORMANCE OF THE CONTRACT.

MAINTENANCE

ALL WATERWAYS SHALL BE CLEARED AS SOON AS PRACTICABLE OF FALSEWORK, PILING, DEBRIS OR OTHER OBSTRUCTIONS PLACED DURING CONSTRUCTION OPERATIONS AND NOT A PART OF THE FINISHED WORK.

SEDIMENTATION COLLECTED IN THE DETENTION BASIN SHALL BE REMOVED AFTER IT HAS BUILT UP TO THE ELEVATION OF THE BEGINNING OF THE PERFORATIONS IN THE RISER PIPE. ALL SILT COLLECTED SHALL BE PLACED BACK UP ON THE SITE.

SEEDS AREAS THAT HAVE WASHED AWAY SHALL BE FILLED AND GRADED, AS NECESSARY, AND THEN RESEED. ADDITIONAL HAY MULCHING SHALL BE APPLIED TO RETAIN THE SEED UNTIL IT HAS A CHANCE TO ROOT PROPERLY.

AREAS THAT CONTAIN SOD SHALL BE CHECKED VERY CAREFULLY TO ENSURE THAT JOINTS BETWEEN THE SOD STRIPS ARE TIGHT AND SECURE. WHERE JOINT SEPARATION IS IN EVIDENCE, A CAREFUL INSPECTION OF EACH JOINT SHALL BE MADE TO DETERMINE WHETHER UNDERMINING OF THE STRIPS IS OCCURRING. IF IT IS, THE STRIPS SHALL BE ROLLED UP, THE SUBSURFACE SHALL BE FILLED AND GRADED, AS REQUIRED, AND THE SOD STRIPS SHALL BE RELAID WITH THE PROPER JOINTS AND PEGGING.

THE ABOVE PROCEDURE FOR SEEDING AND SODDED AREAS SHALL BE REPEATED AFTER EACH RUNOFF PRODUCING RAIN UNTIL NO MORE SIGNS OF EROSION ARE EVIDENT.

A GOOD STAND OF VEGETATION SHALL BE DEFINED AS THE MANUFACTURER'S RECOMMENDED HEIGHT. SHOULD UNFORESEEN EROSION CONDITIONS DEVELOP DURING CONSTRUCTION, THE CONTRACTOR SHALL TAKE ACTION TO REMEDY SUCH CONDITIONS AND TO PREVENT DAMAGE TO ADJACENT PROPERTIES AS A RESULT OF INCREASED RUNOFF AND/OR SEDIMENT DISPLACEMENT. SPECIAL ATTENTION SHOULD BE GIVEN TO FROZEN SLOPES. STOCKPILES OF WOOD CHIPS, HAY BALES, CRUSHED STONE AND OTHER MULCHES SHALL BE HELD IN READINESS TO DEAL IMMEDIATELY WITH EMERGENCY PROBLEMS OF EROSION.

DURING CONSTRUCTION THE CONTRACTOR WILL BE RESPONSIBLE FOR SOIL EROSION CONTROL MAINTENANCE. AFTER CONSTRUCTION THE OWNER WILL BE RESPONSIBLE FOR MAINTAINING THE FACILITIES.

TEMPORARY EROSION AND SEDIMENTATION CONTROL

TEMPORARY INLET PROTECTION SHALL BE APPLIED TO EACH INLET. THEY SHALL REMAIN IN PLACE UNTIL A GOOD STAND OF VEGETATION HAS ESTABLISHED.

SEED SHALL BE APPLIED ACCORDING TO THE ENCLOSED CHART. THE CONTRACTOR SHALL ADHERE TO THE RECOMMENDED SEEDING DATES WHEN SELECTING TEMPORARY SEED TYPES. FERTILIZE, LIME AND MULCH IN ACCORDANCE WITH RATES AND MATERIALS INDICATED IN PERMANENT STABILIZATION SECTION.

TEMPORARY SEEDINGS AND SEEDING DATES

CROP	SEEDING RATES		SEEDING DATES SOUTH JERSEY
	PER ACRE	PER 1000 S.F.	
OATS (SPRING)	86 LBS.	2 LBS.	BEFORE APR. 1
RYE (WINTER CEREAL)	112 LBS.	2.8 LBS.	AUG. 1-NOV. 15
BARLEY (WINTER)	96 LBS.	2.2 LBS.	AUG. 15-OCT. 1
RYEGRASSSES (PERENNIAL)	100 LBS.	1.0 LBS.	BEFORE MAY 15 AUG. 15-OCT. 1
MILLET (GERMAN OR HUNGARIAN)	30 LBS.	0.5 LB.	MAY 1-AUG. 15
MILLET (PEARL)	20 LBS.	0.7 LB.	MAY 1-AUG. 15
WEeping LOVEGRASS	3 LBS.	0.1 LB.	MAY 1-AUG. 15

DUST CONTROL SHALL BE ACCOMPLISHED BY PERIODIC SPRINKLING OF DISTURBED AREAS, AS REQUIRED.

PERMANENT STABILIZATION

ALL EXPOSED EARTH SURFACES SHALL BE PERMANENTLY STABILIZED AFTER COMPLETION OF CONSTRUCTION OF ALL SITE IMPROVEMENTS.

PERMANENT STABILIZATION SHALL BE ACCOMPLISHED BY THE FOLLOWING MATERIALS AND METHODS.

MATERIALS	TYPE	APPLICATION RATES
LIME	PULVERIZED DOLOMATIC 50% CaO	2 TONS/ACRE
FERTILIZER	P ₀ K ₀ I ₀	600 LB./ACRE
SEED	1) TURFTYPE TALL FESCUE 2) KENTUCKY BLUEGRASS 3) TURFTYPE PERENNIAL RYEGRASS	80% BY WEIGHT 208 LB./ACRE 10% BY WEIGHT 28 LB./ACRE 10% BY WEIGHT 28 LB./ACRE
MULCH	STRAW MULCH	1.5 TO 2 TONS/ACRE
MULCH BINDER:	ORGANIC AND VEGETABLE BASED BINDERS	AS RECOMMENDED BY MANUFACTURER

METHODS

1. WORK LIME AND FERTILIZER INTO SOIL WITH A SPRING TOOTH OR DISC HARROW.
2. APPLY SEED AT THE GIVEN RATE AND FIRM WITH A ROLLER OR LIGHT DRAG.
3. APPLY STRAW MULCH IN AN UNIFORM FASHION UNTIL 80% OF THE GROUND IS COVERED; A SLIGHTLY HEAVIER APPLICATION IS NECESSARY ON STEEP SLOPES.
4. ANCHOR STRAW MULCH IN PLACE BY SPRAYING MULCH BINDER AT GIVEN RATES.

HYDROSEEDING : OPTIMUM GROWING SEASONS AND FLAT SLOPES ONLY.

- (A) HYDRO SEEDING SHALL NOT BE PERMITTED FOR PERMANENT STABILIZATION OF THE MULTI-PURPOSE FIELD.
- (B) AS AN ALTERNATIVE TO THE PRECEDING, AREAS TO RECEIVE PERMANENT VEGETATIVE COVER CAN BE HYDROSEEDED USING THE PREVIOUS FERTILIZATION, SEEDING, AND LIMING RATES.
- (C) PAPER MULCH: 1500 LBS/ACRE NOT TO BE MIXED IN TANK WITH SEED SLURRY.
- (D) SLOPED AREAS OR SEEDING OUTSIDE THE OPTIMUM FALL AND SPRING SEASONS WILL REQUIRE THE APPLICATION OF STRAW MULCH AND MULCH ANCHOR, 70-90 LBS/1000 S.F.

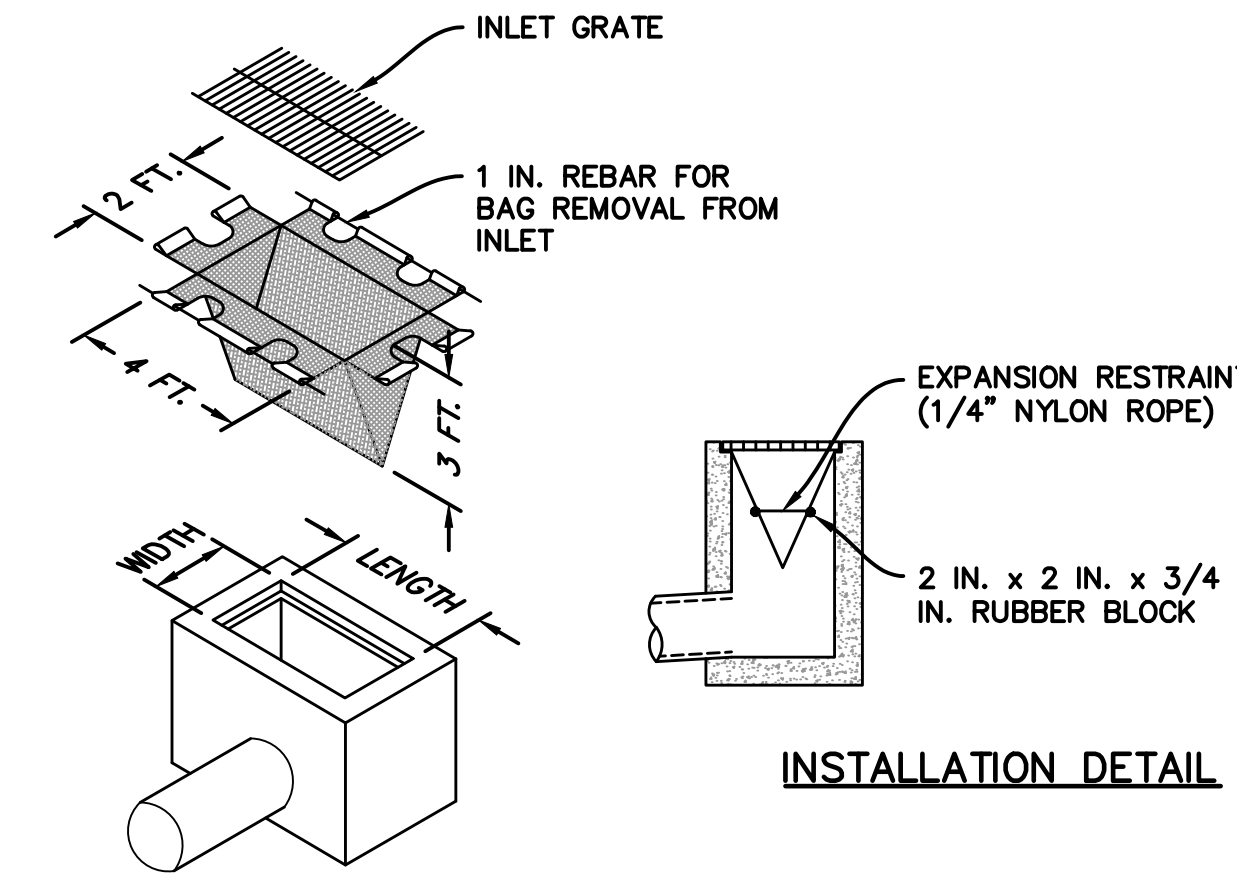
4" OF CLEAN TOP SOIL SHALL BE PROVIDED ON ALL AREAS OF DISTURBANCE WHERE TOP SOIL DOES NOT EXIST.

SOIL EROSION AND SEDIMENT CONTROL NOTES

1. ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES TO BE INSTALLED PRIOR TO ANY MAJOR SOIL DISTURBANCES, OR IN THEIR PROPER SEQUENCE, AND MAINTAINED UNTIL PERMANENT PROTECTION IS ESTABLISHED.
2. ANY DISTURBED AREAS THAT WILL BE LEFT EXPOSED MORE THAN 30 DAYS AND NOT SUBJECT TO CONSTRUCTION TRAFFIC WILL IMMEDIATELY RECEIVE A TEMPORARY SEEDING. IF THE SEASON PREVENTS THE ESTABLISHMENT OF TEMPORARY COVER, THE DISTURBED AREAS WILL BE MULCHED WITH STRAW OR EQUIVALENT MATERIAL, AT A RATE OF TWO (2) TONS PER ACRE, ACCORDING TO STATE STANDARDS.
3. PERMANENT VEGETATION TO BE SEEDING OR SODDED ON ALL EXPOSED AREAS WITHIN TEN (10) DAYS AFTER FINAL GRADING. MULCH WILL BE USED FOR PROTECTION UNTIL SEEDING IS ESTABLISHED.
4. SOIL EROSION AND SEDIMENT CONTROL PRACTICES ON THE PLAN SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL IN NEW JERSEY.
5. A SUB-BASE COURSE WILL BE APPLIED IMMEDIATELY FOLLOWING ROUGH GRADING AND INSTALLATION OF IMPROVEMENTS IN ORDER TO STABILIZE STREETS, ROADS, DRIVEWAYS AND PARKING AREAS. IN AREAS WHERE NO UTILITIES ARE PRESENT, THE SUB-BASE SHALL BE INSTALLED WITHIN 15 DAYS OF PRELIMINARY GRADING.
6. IMMEDIATELY FOLLOWING INITIAL DISTURBANCE OR ROUGH GRADING ALL CRITICAL AREAS SUBJECT TO EROSION (i.e.: STEEP SLOPES, ROADWAY EMBANKMENTS) WILL RECEIVE A TEMPORARY SEEDING IN COMBINATION WITH STRAW MULCH OR A SUITABLE EQUIVALENT, AT A RATE OF TWO (2) TONS PER ACRE, ACCORDING TO STATE STANDARDS.
7. ANY STEEP SLOPES RECEIVING PIPELINE INSTALLATION WILL BE BACKFILLED AND STABILIZED DAILY, AS THE INSTALLATION PROCEEDS (i.e.: SLOPES GREATER THAN 3:1)
8. A CRUSHED STONE TIRE CLEANING PAD WILL BE INSTALLED WHEREVER A CONSTRUCTION ACCESS EXISTS. THE RIP RAP PAD MUST BE 100 FEET IN LENGTH AND THE STONE MUST BE 1.5"-4" IN SIZE, PLACED 12" THICK AND FULL WIDTH OF THE ENTRANCE. IT SHOULD BE UNDERLAIN WITH A SUITABLE SYNTHETIC FILTER FABRIC AND MAINTAINED.
9. IN ACCORDANCE WITH THE STANDARDS FOR PERMANENT VEGETATIVE COVER FOR SOIL STABILIZATION, ANY SOIL HAVING A PH OF 4 OR LESS OR CONTAINING IRON SULFIDES SHALL BE COVERED WITH A MINIMUM OF 12 INCHES OF SOIL HAVING A PH OF 5 OR MORE PRIOR TO SEEDBED PREPARATION.
10. THE GLOUCESTER COUNTY SOIL CONSERVATION DISTRICT SHALL BE NOTIFIED 72 HOURS IN ADVANCE OF ANY LAND DISTURBING ACTIVITY.
11. AT THE TIME WHEN THE SITE PREPARATION FOR PERMANENT VEGETATIVE STABILIZATION IS GOING TO BE ACCOMPLISHED, ANY SOIL THAT WILL NOT PROVIDE A SUITABLE ENVIRONMENT TO SUPPORT ADEQUATE VEGETATIVE GROUND COVER SHALL BE REMOVED OR TREATED IN SUCH A WAY THAT WILL PERMANENTLY ADJUST THE SOIL CONDITIONS AND RENDER IT SUITABLE FOR VEGETATIVE GROUND COVER. IF THE REMOVAL OR TREATMENT OF THE SOIL WILL NOT PROVIDE SUITABLE CONDITIONS, NON-VEGETATIVE MEANS OF PERMANENT GROUND STABILIZATION WILL HAVE TO BE EMPLOYED.
12. PRIOR TO THE COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL REQUEST, IN WRITING, A FINAL INSPECTION BY THE SOIL CONSERVATION DISTRICT. A SATISFACTORY REPORT OF COMPLIANCE IS MANDATORY PRIOR TO ANY PROJECT BEING DEEMED COMPLETE. (PURSUANT TO N.J.S.A. 4:24-39 ET SEQ.)
13. CONDUIT OUTLET PROTECTION MUST BE INSTALLED AT ALL REQUIRED OUTFALLS PRIOR TO THE DRAINAGE SYSTEM BECOMING OPERATIONAL.
14. ANY CHANGES TO THE CERTIFIED SOIL EROSION AND SEDIMENT CONTROL PLANS WILL REQUIRE THE SUBMISSION OF REVISED SOIL EROSION AND SEDIMENT CONTROL PLANS TO THE DISTRICT FOR RE-CERTIFICATION. THE REVISED PLANS MUST MEET ALL CURRENT STATE SOIL EROSION AND SEDIMENT CONTROL STANDARDS.
15. ANY CONVEYANCE OF THIS PROJECT PRIOR TO ITS COMPLETION WILL TRANSFER FULL RESPONSIBILITY FOR COMPLIANCE WITH THE CERTIFIED PLAN TO ALL SUBSEQUENT OWNERS.
16. OFFSITE SEDIMENT DISTURBANCE MAY REQUIRE ADDITIONAL CONTROL MEASURES TO BE DETERMINED BY THE EROSION CONTROL INSPECTOR.
17. A COPY OF THE CERTIFIED SOIL EROSION AND SEDIMENT CONTROL PLAN MUST BE MAINTAINED ON THE PROJECT SITE DURING CONSTRUCTION.
18. THE SOIL EROSION CONTROL INSPECTOR MAY REQUIRE ADDITIONAL SOIL EROSION MEASURES TO BE INSTALLED, AS DIRECTED BY THE DISTRICT INSPECTOR.
19. ALL SEDIMENTATION STRUCTURES WILL BE INSPECTED AND MAINTAINED ON A REGULAR BASIS AND AFTER EVERY STORM EVENT.
20. ALL APPLICABLE EROSION CONTROL PRACTICES SHALL BE LEFT IN PLACE UNTIL CONSTRUCTION IS COMPLETED AND/OR THE AREA IS STABILIZED.
21. THE SITE SHALL AT ALL TIMES BE GRADED AND MAINTAINED SUCH THAT ALL STORMWATER RUNOFF IS DIVERTED TO SOIL EROSION AND SEDIMENT CONTROL FACILITIES.
22. PAVED ROADWAYS MUST BE KEPT CLEAN AT ALL TIMES.
23. IT SHALL BE RESPONSIBILITY OF THE DEVELOPER TO PROVIDE CONFIRMATION OF LIME, FERTILIZER AND SEED APPLICATION RATES AT THE REQUEST OF THE BURLINGTON COUNTY SOIL CONSERVATION DISTRICT.
24. MAXIMUM SIDE SLOPES OF ALL EXPOSED SURFACES SHALL NOT BE CONSTRUCTED STEEPER THAN 3:1.
25. ALL CRITICAL AREAS SUBJECT TO EROSION WILL RECEIVE A TEMPORARY SEEDING IN COMBINATION WITH STRAW MULCH IN ACCORDANCE WITH THE STANDARDS IMMEDIATELY FOLLOWING ROUGH GRADING.
26. SOIL STOCKPILES ARE NOT TO BE LOCATED WITHIN FIFTY (50) FEET OF A FLOODPLAIN, SLOPE, ROADWAY OR DRAINAGE FACILITY. THE BASE OF ALL STOCKPILES SHOULD BE PROTECTED BY A HAY BALE BARRIER OR SEDIMENT FENCE.
27. ALL CATCH BASIN INLETS WILL BE PROTECTED DURING CONSTRUCTION (FILTER DETAILS APPEAR ON PLAN).
28. ALL DEWATERING OPERATIONS MUST DISCHARGE DIRECTLY INTO A SEDIMENT FILTRATION DEVICE. THE SEDIMENT FILTER MUST BE PLACED SO AS NOT TO CAUSE EROSION OF THE DOWNSTREAM AREA. FIELD PLACEMENT AND USE OF THE STRUCTURE MUST BE APPROVED BY THE DISTRICT EROSION CONTROL INSPECTOR PRIOR TO COMMENCEMENT OF DEWATERING ACTIVITIES.
29. IMMEDIATELY AFTER THE COMPLETION OF STRIPPING AND STOCKPILING TOPSOIL, THE STOCKPILE SHALL BE SEEDING WITH TEMPORARY VEGETATION. STABILIZE TOPSOIL STOCKPILES WITH STRAW MULCH FOR PROTECTION IF THE SEASON DOES NOT PERMIT THE ESTABLISHMENT OF TEMPORARY SEED.

CONSTRUCTION SEQUENCE

- | | |
|---|--------|
| 1. PLACEMENT OF SOIL EROSION AND SEDIMENTARY CONTROL MEASURES | 1 DAY |
| 2. INSTALL CONCRETE SIDEWALK AND CURB | 1 WEEK |
| 3. SAW CUT PAVEMENT AND MILL ROADWAY IN ACCORDANCE WITH THE PLANS | 1 WEEK |
| 4. INSTALL STORM MAINS IN ACCORDANCE WITH THE PLANS. | 3 DAYS |
| 5. INSTALL HMA SURFACE COURSE IN ACCORDANCE WITH GRADING DESIGN | 2 DAYS |
| 6. RESTORE DISTURBED GRASSED AREAS | 1 DAY |
| 7. REMOVE SOIL AND EROSION AND SEDIMENTATION CONTROL MEASURES ONLY AFTER DISTURBED AREAS ARE STABILIZED | 1 DAY |



ISOMETRIC VIEW

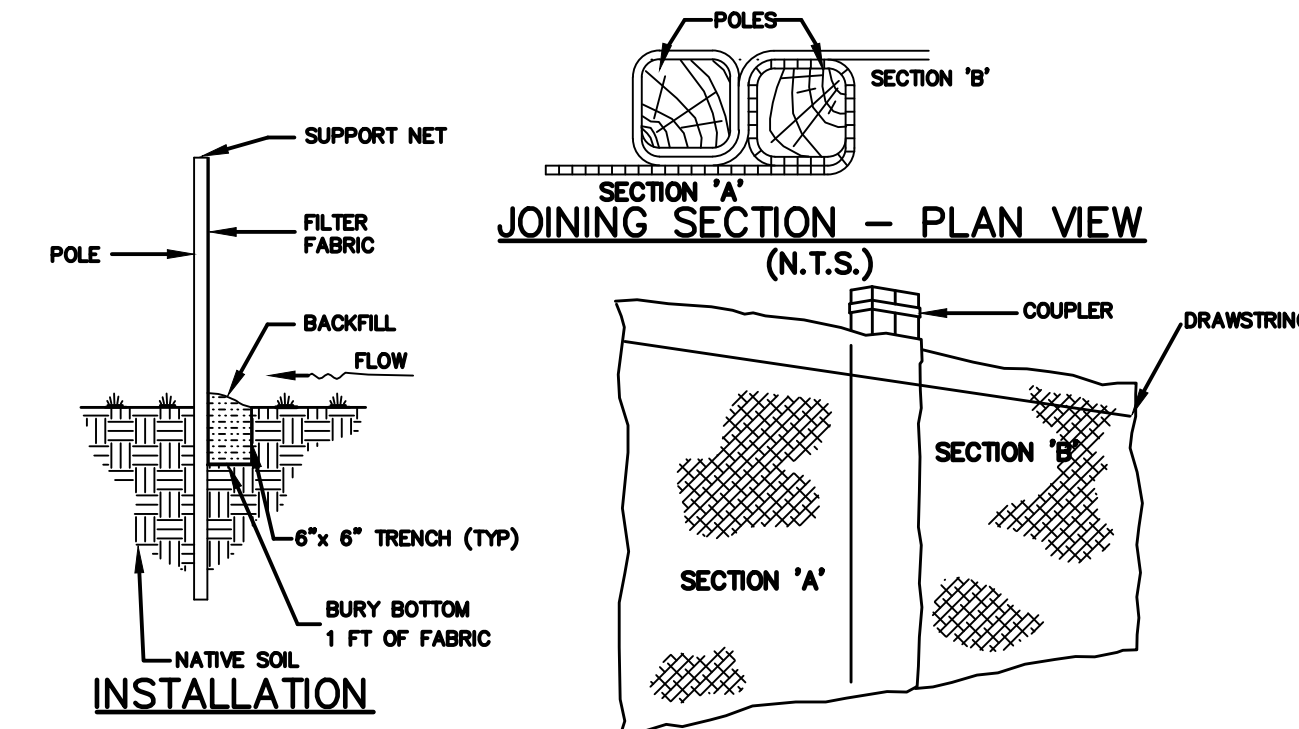
NOTES:

MAXIMUM DRAINAGE AREA = 1/2 ACRE.

AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS., A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZODAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40 SIEVE.

INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION.

INLET FILTER BAG PROTECTION DETAIL N.T.S.



SILT FENCE TO BE USED ON SITE SHALL BE "ENVROFENCE" PRECONSTRUCTED SILT FENCE OR EQUAL. SPECIFICATIONS ARE: LENGTH: 100 FT./FABRIC WIDTH: 3 FT./POLE LENGTH: 4.5 FT./POLE SPACING: 7.7 FT./INSTALLED HEIGHT: 2.5 FT./SEDIMENT FABRIC: MIRAFL 100/DRAWSTRING THROUGH FABRIC ALONG TOP OF FENCE.

NOTE: THE FABRIC MUST BE SECURELY FASTENED TO THE POSTS USING A SYSTEM CONSISTING OF METAL FASTENERS (NAILS AND STAPLES) AND A HIGH STRENGTH REINFORCEMENT MATERIAL (NYLON WEBBING, GROMMETS, WASHERS, ETC.) PLACED BETWEEN THE FASTENER AND THE GEOTEXTILE FABRIC.

SILT FENCE DETAIL N.T.S.

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 WILLINGBORO TOWNSHIP, BURLINGTON COUNTY, NEW JERSEY

SOIL EROSION SEDIMENT CONTROL NOTES

WILLINGBORO TOWNSHIP
 1 REV DR MARTIN LUTHER KING JR RD
 WILLINGBORO, NJ 08046

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PROJECT	WTCEX21028
DATE	JANUARY 10, 2022
DRAWING SCALE	
DRAWN BY	SMR
APPROVED BY	HJD

FOR SOIL EROSION AND SEDIMENT CONTROL PURPOSES ONLY

PLOT TITLE: PENNONI ASSOCIATES
 PROJECT: BUCKINGHAM DRIVE CULVERT
 DRAWN BY: SMR
 CHECKED BY: HJD
 DATE: 01/10/22
 SCALE: AS SHOWN
 SHEET: 8 OF 7